

ORDINANCE NO. 868

AN ORDINANCE AMENDING THE CITY OF STAYTON'S COMPREHENSIVE PLAN, CHAPTER 3, RELATING TO THE TRANSPORTATION SYSTEM PLAN; AND, DECLARING AN EMERGENCY.

WHEREAS, the City of Stayton has an established Comprehensive Plan, which includes provisions addressing transportation matters;

WHEREAS, the Stayton City Council, following public hearings conducted by the Stayton Planning Commission and the Stayton City Council, desires to amend Chapter 3 of Stayton's Comprehensive Plan in respect to transportation system matters; and,

WHEREAS, it is appropriate that an emergency be declared as to the enactment of this Ordinance so that it is in full force and effect immediately from and after its adoption by the Stayton City Council and the Mayor's signing.

NOW, THEREFORE, the Stayton City Council does ordain as follows:

SECTION 1. Chapter 3, Stayton Comprehensive Plan, is amended and restated as follows:

CHAPTER 3

Transportation System Plan

The City of Stayton, Comprehensive Plan, Chapter 3, addresses the City of Stayton's anticipated transportation needs through 2025. It has been prepared to meet state and federal regulations that require urban areas to conduct long-range planning. Specifically, the TSP was developed in compliance with requirements of the Transportation Equity Act for the 21st Century (TEA-21), Statewide Planning Goal 12, the Transportation Planning Rule (TPR - Oregon Administrative Rule (OAR) Chapter 660, Division 12), and Oregon Highway Plan (1999). The long range planning is intended to serve as a guide for the City of Stayton in managing their existing transportation facilities and developing future transportation facilities.

Please refer to the 2004 Stayton Transportation System Plan document for transportation - related information. The remainder of this chapter identifies transportation facilities and reiterates the transportation goals, and policies in the TSP.

The transportation element of the Stayton Comprehensive Plan considers ways to provide a safe, convenient, efficient, and economic system of moving people and goods in, around, and through the Stayton area. The modes of transportation to be considered under the transportation goal are: A) mass transit; B) rail; C) air; D) water; E) pipeline; and F) non-motorized improvements. The transportation element also considers the transportation disadvantaged. The streets and highways section address the items required in OAR 660, Div. 11, the public facilities rule.

Transportation Facilities

A. Mass Transit

Mass transit is passenger transportation which carries members of the public on a regular and continuing basis. Buses, taxis, shuttle trains, and car pools are forms of mass transit. As the cost of travel by private automobile increases, the alternative modes of mass transit, including rail and bus facilities, become more of an economic possibility.

Oregon Bus Lines (tickets through Greyhound Bus Lines) provides bus service from Salem to Bend via Highway 22. Stayton may benefit from a commuter system to and from the Salem area within the next decade. An intra-city bus system may also become feasible as growth continues and the cost of operating the private auto rises.

Taxi service is available from Salem; however, the cost to an individual is high. At present, there is no taxi service available in the Stayton area. This form of transportation will not be readily available until the population of the Stayton area reaches a level that can support a taxi service.

The most practical form of mass transit is sharing of an automobile. This is becoming an attractive alternative for several reasons: cost of operation, reduced traffic, and less need for parking facilities at major employment centers. The Stayton park-and-ride lot is located on State Highway Division land on the southeast corner of the intersection of Cascade Highway and Highway 22. The Mid-Willamette Valley Council of Governments has developed a car pool program with the State of Oregon and the City of Salem. Individuals in the Stayton area can receive a list of persons interested in sharing a ride by contacting the car pool program. The telephone number is 585-POOL.

B. Railroad

At present, there is a rail spur to Stayton from the Southern Pacific mainline in Salem. The spur terminates at NORPAC Foods, Inc., formerly the Stayton Cooperative Cannery. Wilco Farmers and Trus-Joist also maintain sidings to benefit from this spur. The other industrial areas along the spur could also become a major user of these rail facilities as the need for rapid and inexpensive movement of bulky items increases. An old railroad spur was removed that served Karsten and Philips Industries. The removal of this line allows the future extension of Locust Street to be constructed without a grade crossing.

C. Air Transportation

The City of Stayton does not have an airport. There is not a sufficient need to support an airport at this time, nor is there a good airport site within the Stayton UGB. There is a full service commercial airport 15 miles away in Salem that provides needed service. Several small private air strips in Marion and Linn counties are within 20 miles of Stayton. A heliport at Santiam Hospital provides for recreational and medical emergencies.

D. Water Transportation

Stayton is located adjacent to the North Santiam River and has developed around the need and demands of water oriented industries. The river has not been utilized as a mode of transportation because it is fairly shallow and other modes have been more economical. It is possible to travel by water from Stayton to Jefferson and the Willamette River; however, there are more economical and timely methods of travel. The river will continue to be used for aesthetic and recreational values and protected as a source of drinking water. It is doubtful if other than small recreational craft will ever travel on the river.

E. Pipeline Facilities

The only existing pipeline facilities are the city water system and the natural gas system. The water system is discussed in detail in the Public Facilities section of this plan. In addition to Stayton, the City of Salem transmits potable water from their supply facilities on Stayton (Geren) Island via two large transmission mains. The natural gas system is discussed in the Energy section of this plan.

An additional pipeline facility may, at some future date, be located in the Stayton area. The U.S. Forest Service is issuing exploratory permits for geothermal energy drilling in the Breitenbush Hot Springs area near Detroit. If and when sufficient geothermal resources are found and developed, Stayton will become a logical site for the receipt and use of this resource as an economical energy supply.

F. Non-Motorized Improvements

Future non-motorized improvements development includes sidewalks, bike lanes, and trails. The sidewalk, bike lane improvements, and pedestrian trails are identified in Section 1.0, Figures 1-3 and 1-4 of the TSP. Most of the sidewalk improvements outlined in the Stayton TSP will be implemented with street widening projects, local improvement districts (LID's) or when other funding becomes available. Bicycle improvements will most likely take place during the widening of streets.

Transportation Goals and Policies

This section establishes broad policy objectives that provide the context to make transportation investment decisions and to develop the existing and future transportation system within the City of Stayton urban growth boundary.

A. GOAL 1 - MOBILITY

It is the goal of the City of Stayton to provide a multi-modal transportation system that maximizes the mobility of Stayton residents and businesses.

The policies to be used to implement Goal 1 - Mobility are as follows:

- 1.1. Establish a transportation system that can accommodate a wide variety of travel modes and minimizes the reliance on any one single mode of travel.

- 1.2. Properly plan transportation infrastructure to meet the level of service set for each type of facility.
- 1.3. Maintain a minimum level of service standard of LOS D for signalized intersections. Maintain a minimum level of service standard of LOS D for all way stop controlled intersections and roundabouts. Maintain a minimum level of service standard of LOS E or F with a volume to capacity of 0.95 or better for two-way stop controlled intersections.

For Oregon Department of Transportation (ODOT) facilities, the City of Stayton shall defer to ODOT mobility standards described in the most recent version of the Oregon Highway Plan.

- 1.4. Develop a local street plan to preserve future rights-of-way for future streets and to maintain adequate local circulation in a manner consistent with Stayton's existing street grid system.
- 1.5. Require developments to construct their accesses consistent with Stayton's existing street grid system.
- 1.6. Develop an access management policy for the local arterial system and direct commercial development access to local streets wherever possible.

B. GOAL 2 – EFFICIENCY

It is the goal of the City of Stayton to create and maintain a multi-modal transportation system with the greatest efficiency of movement possible for Stayton residents and businesses in terms of travel time, travel distance, and efficient management of the transportation system.

The policies to be used to implement Goal 2 - efficiency are as follows:

- 2.1. Develop the City of Stayton's transportation system with alternative parallel corridors to reduce reliance on any one corridor and improve local access through a local street plan that preserves future rights-of-ways, consistent with a grid pattern.
- 2.2. Plan and improve routes to facilitate the movement of goods and services.
- 2.3. Manage the City of Stayton's resources to improve the transportation system through an up-to-date Capital improvement program reflecting the transportation needs of the city.
- 2.4. Encourage development to occur near existing community centers where services are presently available to minimize the need for expanding services and to more efficiently utilize existing resources.

C. GOAL 3 - SAFETY

It is the goal of the City of Stayton to maintain and improve transportation system safety.

The policies to be used to implement Goal 3 - Safety are as follows:

- 3.1. Examine the need for speed reduction in specific areas such as adjacent to local schools.
- 3.2. Ensure that the multi-modal transportation system within Stayton is structurally and operationally safe.
- 3.3. Periodically review crash records in an effort to systematically identify and remedy unsafe intersection and roadway locations.
- 3.4. Develop a traffic calming program for implementation in areas with vehicle speeding issues.
- 3.5. Ensure adequate access for emergency services vehicles throughout the city's transportation system.

D. GOAL 4 - EQUITY

It is the goal of the City of Stayton to ensure the cost of transportation infrastructure and services are borne by those who benefit from them. The policies to be used to implement Goal 4 - Equity are as follows:

- 4.1. System Development Charges (SDC) shall be updated periodically to accurately reflect a nexus between the traffic impact of development and the fees assessed to the development.
- 4.2. The City of Stayton shall seek equitable funding mechanisms to maintain transportation infrastructure and services at an acceptable level.
- 4.3. Developments shall be responsible for mitigating their direct traffic impacts. These impacts shall be determined through an approved traffic study submitted to the city by the developer.

E. GOAL 5 - ENVIRONMENTAL

It is the goal of the City of Stayton to limit and mitigate adverse environmental impacts associated with traffic and transportation system development.

The policies to be used to implement Goal 5 - Environmental are as follows:

- 5.1. Transportation project related environmental impacts shall be identified at the earliest opportunity to ensure compliance with all federal and state environmental standards.
- 5.2. Transportation project environmental impacts shall be mitigated to state and federal standards as appropriate.

F. GOAL 6 - ALTERNATIVE MODES OF TRANSPORTATION

Increase the use of alternative modes of transportation (walking, bicycling, rideshare/carpooling, and transit) through improved access, safety, and service. Increasing the use of alternative transportation modes includes maximizing the level of access to all social, work, and social resources for the transportation disadvantaged. The City of Stayton seeks for its transportation disadvantaged citizens

the creation of customer-oriented regionally coordinated public transit system that is efficient, effective, and founded on present and future needs of the community.

The policies to be used to implement Goal 6 - Alternative Modes of Transportation are as follows:

- 6.1. Develop a citywide pedestrian and bicycle plan providing for sidewalks, bikeways, and safe crossings.
- 6.2. Promote alternative modes and rideshare/carpool programs through community awareness and education.
- 6.3. Coordinate with regional transit service efforts.
- 6.4. Seek Transportation and Growth Management (TGM) and other funding for projects evaluating and improving the environment for alternative modes of transportation.
- 6.5. Seek improvements of mass transit services to the City of Stayton.
- 6.6. Transportation Disadvantaged:
 - a. Continue to support programs for the transportation disadvantaged where such programs are needed and are economically feasible.
 - b. Increase all citizens' transportation disadvantaged where such programs are needed and are economically feasible.
 - c. Identify and retain community identity and autonomy.
 - d. Create a customer-oriented focus in the provision of transportation services.
 - e. Hold any regional system accountable for levels and quality of service.
 - f. Enhance public transportation sustainability.
 - g. Promote regional planning of transportation services.
 - h. Use innovative technology to maximize efficiency of operation, planning, and administration of public transportation.
 - i. Promote both inter-community and intra-community transportation services for the transportation disadvantaged.

G. GOAL 7 - MAINTAIN MULTI-JURISDICTION COORDINATION

Maintain coordination between the City of Stayton, Marion County, and the Oregon Department of Transportation (ODOT).

The policies to be used to implement Goal 7 - Maintain Multi-Jurisdictional Coordination are as follows:

- 7.1. Cooperate with ODOT in the implementation of the State wide Transportation Improvement Program (STIP).
- 7.2. Encourage improvement of state highways, especially Highway 22 in the vicinity of Golf Club Road, Cascade Highway, Fern Ridge Road, and Stayton Road.
- 7.3. Work with Marion and Linn Counties ODOT, and the City of Sublimity in establishing cooperative road improvement programs and schedules.
- 7.4. Work to establish the right-of-way needed for new roads identified in the TSP.
- 7.5. Take advantage of federal and state highway funding programs.
- 7.6. Coordinate with ODOT to complete Phase 2 of the ORE 22, Joseph Street to Stayton project.

H. GOAL 8 - ROADWAY FUNCTIONAL CLASSIFICATION

It is the goal of the City of Stayton to properly plan and maintain its transportation system based on a roadway functional classification system. The street and access standards are based on this roadway functional classification system.

The policies to be used to implement Goal 8 - Roadway Functional Classification are as follows:

- 8.1. The transportation system plan (TSP) shall classify roadways throughout the city's transportation system. Both an arterial and local street classification shall be identified in the TSP.
- 8.2. The street and access standards shall employ the roadway functional classification system.
- 8.3. Encourage use of alternative methods, such as alleys, shared driveways, etc., i.e. smart development techniques, to provide property access.
- 8.4. The roadway functional classification system represents a continuum in which through traffic increases and access provisions decrease in the higher classification category. The street and access standards shall reflect this principal.

I. GOAL 9 - TRUCK ROUTE

It is the goal of the City of Stayton to identify and designate a through truck route system utilizing arterial and major collector roads and to minimize impacts to residential areas.

The polices to be used to implement Goal 9 - Truck Route are as follows:

- 9.1. The City of Stayton shall designate a through truck route along its arterials and major collectors. The truck route shall be defined in the TSP.

9.2. Minimize use of other city roadways by truck traffic except by truck traffic for local deliveries and pickups.

9.3 The City of Stayton will follow the applicable state law as it relates to Oregon revised statutes 227.400 truck routes.

J. GOAL 10 - TRANSPORTATION FINANCING

It is the goal of the City of Stayton to seek adequate financial revenues to fund its Capital Improvement Program and maintenance needs.

The policies to be used to implement Goal 10 - Transportation Financing are as follows:

10.1. The City of Stayton shall aggressively seek state and federal funding for relevant transportation projects.

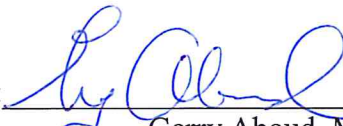
10.2. The City of Stayton shall proactively seek new local and regional funding sources for its Capital Improvement Program.

SECTION 2. Upon adoption by the Stayton City Council and the Mayor's signing, this Ordinance shall become immediately effective.

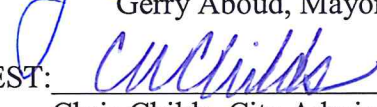
ADOPTED BY THE STAYTON CITY COUNCIL this 20th day of September, 2004.

CITY OF STAYTON

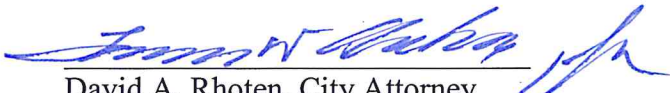
Signed: 9/23, 2004

By: 
Gerry Aboud, Mayor

Signed: 9/23, 2004

ATTEST: 
Chris Childs, City Administrator

APPROVED AS TO FORM:


David A. Rhoten, City Attorney