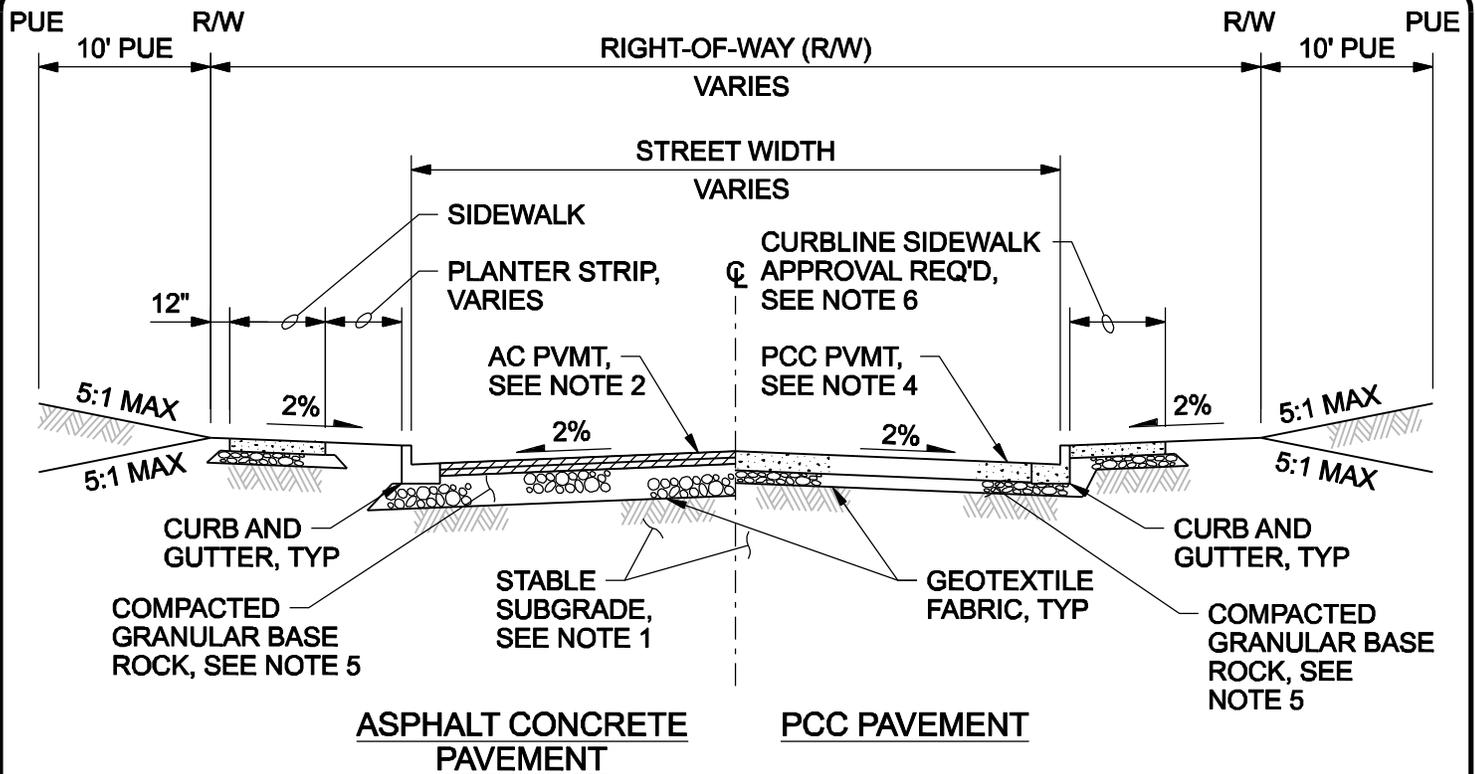


NOTES:

1. "NO PARKING" SIGNS SHALL BE POSTED THE ENTIRE LENGTH OF ALLEY.
2. FIRE HYDRANTS, WHEN REQUIRED, ARE TO BE LOCATED OUTSIDE THE RIGHT-OF-WAY IN A MINIMUM 5' x 5' EASEMENT.
3. SUBGRADE SHALL BE PREPARED, COMPACTED, AND PROOF-ROLLED PRIOR TO PLACEMENT OF GEOTEXTILE AND BASE ROCK, AS SPECIFIED. SUBGRADE SHALL BE PROTECTED FROM DAMAGE DUE TO WEATHER AND CONSTRUCTION TRAFFIC.
4. AC PAVEMENT THICKNESS SHALL BE 4" MINIMUM AND SHALL BE CLASS "C" ASPHALT COMPACTED IN EQUAL LIFTS TO MINIMUM OF 91% OF OPTIMUM PER AASHTO T-209. ALL EDGES AND COMPLETED LIFTS SHALL BE TACK COATED PRIOR TO PAVING. ALL JOINTS SHALL BE SAND SEALED AFTER COMPLETION.
5. BASE ROCK SHALL BE 1"-0" GRANULAR BASE ROCK COMPACTED TO A MINIMUM 95% PER AASHTO T-180.
6. ALL CONCRETE SHALL BE CLASS 5,000 PSI. MAX SLUMP SHALL BE 3" UNLESS OTHERWISE SPECIFIED. ALLEY MAY BE POURED MONOLITHICALLY OR GUTTER SECTIONS MAY BE PLACED SEPARATELY AS SHOWN. IF THE SECTIONS ARE PLACED SEPARATELY, THE CONSTRUCTION JOINT SHALL DOWELED EVERY 2' WITH 1/2" x 18" LONG DEFORMED REBAR DOWELS CENTERED AT THE JOINT.
7. SEE THE STANDARD CONSTRUCTION SPECIFICATIONS FOR INSTALLATION AND SURFACING REQUIREMENTS.
8. AN INVERTED CROWN ALLEY SECTION SHALL BE PROVIDED WHEN REQUIRED BY THE CITY ENGINEER.

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	<p>REVISIONS</p>		<p>302</p>



STREET SECTION	LOCAL STREETS	COLLECTOR STREETS	ARTERIAL STREETS
AC PAVEMENT W/ BASE ROCK	4" AC PVMT, LEVEL 2 10" BASE ROCK	5" AC PVMT, LEVEL 2 12" BASE ROCK	7" AC PVMT, LEVEL 3 15" BASE ROCK
PCC PAVEMENT W/ LEVELING COURSE	7" PCC PVMT 4" BASE ROCK	8" PCC PVMT 4" BASE ROCK	9" PCC PVMT 4" BASE ROCK

TABLE 1 - TYPICAL PAVEMENT STRUCTURE

NOTES:

- SUBGRADE SHALL BE PREPARED, COMPACTED, AND PROOF-ROLLED PRIOR TO PLACEMENT OF GEOTEXTILE AND BASE ROCK, AS SPECIFIED. SUBGRADE SHALL BE PROTECTED FROM DAMAGE DUE TO WEATHER AND CONSTRUCTION TRAFFIC.
- AC PAVEMENT THICKNESS SHALL BE AS SHOWN IN TABLE 1 AND SHALL BE CLASS "C" ASPHALT COMPACTED IN EQUAL LIFTS (3" MAX) TO MINIMUM OF 91% OF OPTIMUM PER AASHTO T-209. ALL EDGES AND COMPLETED LIFTS SHALL BE TACK COATED PRIOR TO PAVING. ALL JOINTS SHALL BE SAND SEALED AFTER COMPLETION.
- DESIGN ENGINEER MAY BE REQUIRED TO SUBMIT ASPHALT PAVEMENT DESIGN DATA FOR ANY STREET CITY ENGINEER HAS REASON TO SUSPECT UNSUITABLE SOIL CONDITIONS, HIGH PERCENTAGE OF TRUCKS, OR ANY OTHER CONDITIONS THAT MAY SIGNIFICANTLY AFFECT THE PAVEMENT DESIGN.
- PCC PAVEMENT SHALL BE 5,000 PSI AT 28 DAY STRENGTH. THICKNESS SHALL BE AS SHOWN IN TABLE 1.
- BASE ROCK SHALL BE 1"-0" GRANULAR BASE ROCK COMPACTED TO A MINIMUM 95% PER AASHTO T-180. BASE ROCK THICKNESS SHALL BE AS SHOWN IN TABLE 1.
- STREET TYPICAL SECTION SHALL HAVE PROPERTY LINE SIDEWALKS, AS SHOWN. CURBLINE SIDEWALKS ARE NOT PERMITTED WITHOUT CITY ENGINEER APPROVAL. SEE STANDARD DRAWING 340.
- FOR TYPICAL UTILITY LOCATIONS, SEE STANDARD DRAWING 202.

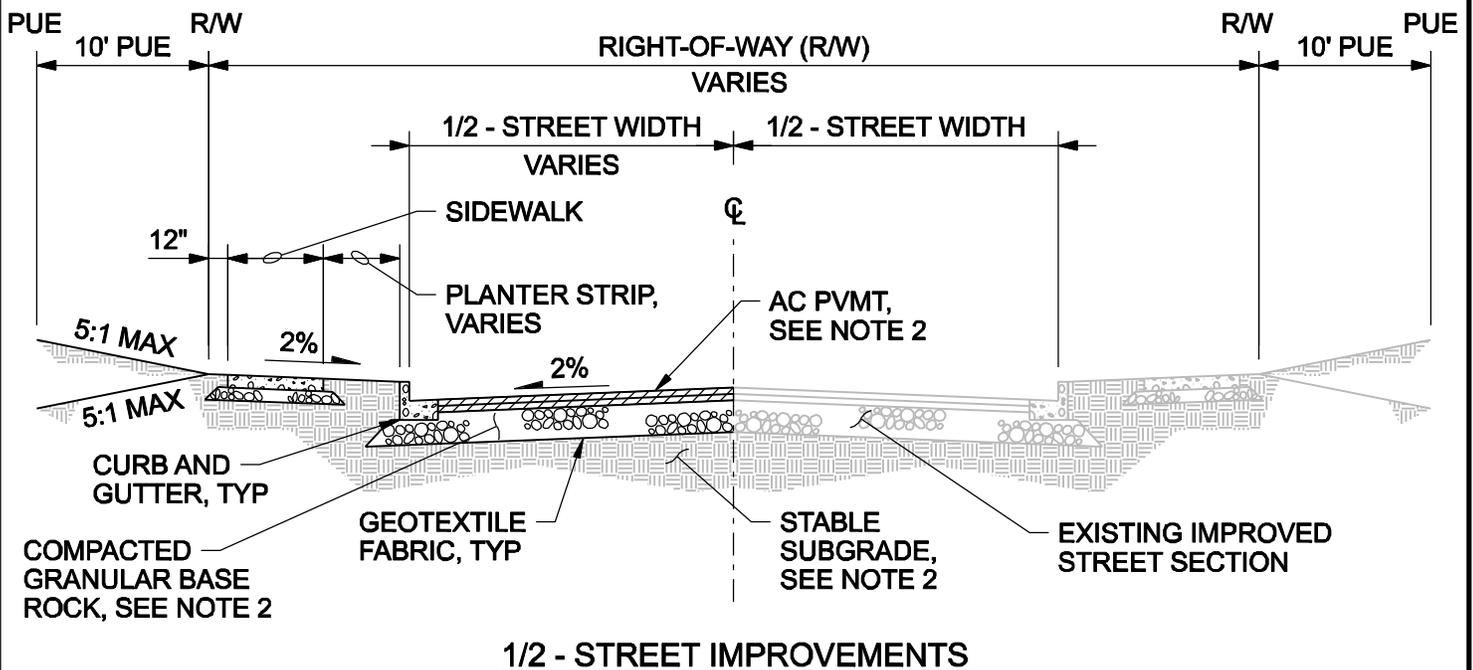
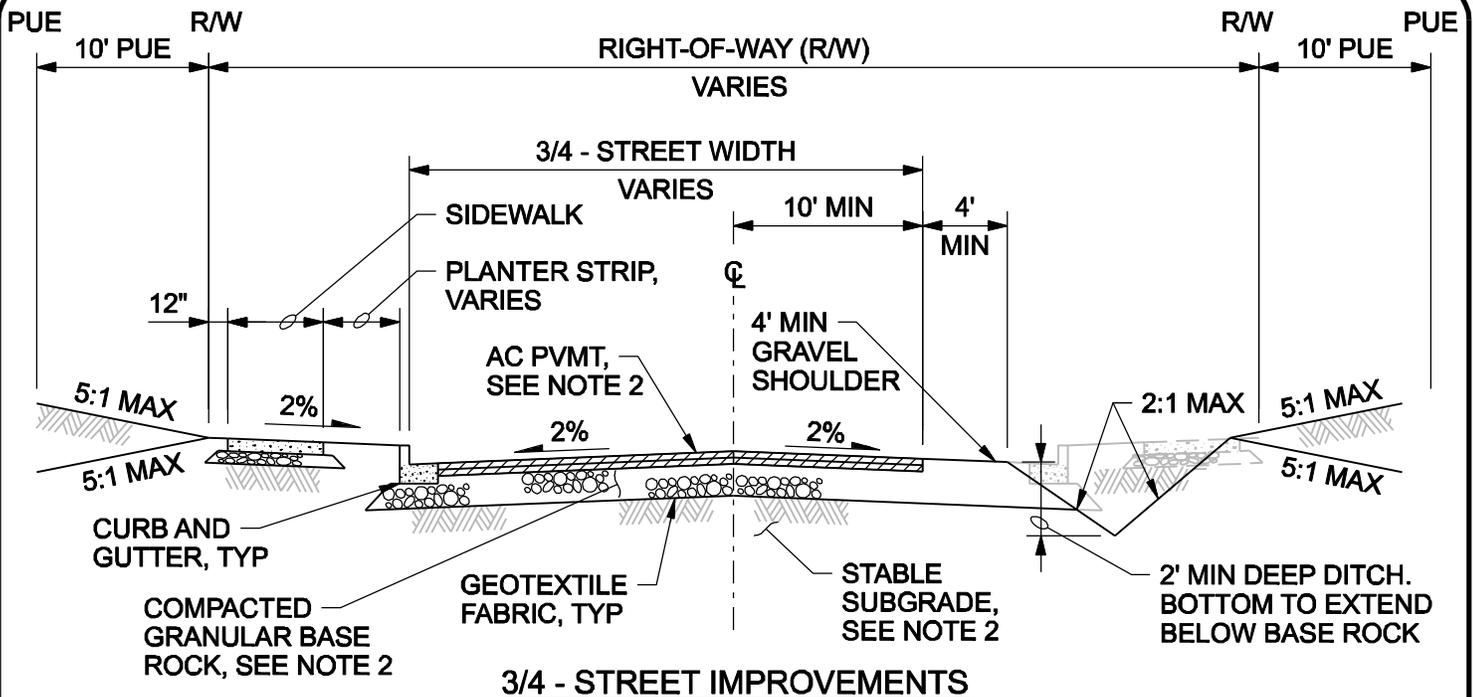


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STANDARD DRAWING TITLE
TYPICAL STREET SECTION

NO SCALE
DIVISION
STREET
DRAWING NO.
304



NOTES:

1. STREET TYPICAL PARTIAL SECTION SHALL HAVE PROPERTY LINE SIDEWALKS, AS SHOWN. CURBLINE SIDEWALKS ARE NOT PERMITTED WITHOUT CITY ENGINEER APPROVAL. SEE STANDARD DRAWING 340.
2. FOR TYPICAL STREET SECTION AND NOTES, SEE STANDARD DRAWING 304.
3. FOR TYPICAL UTILITY LOCATIONS, SEE STANDARD DRAWING 202.
4. THE STORMWATER COLLECTION SYSTEM, INCLUDING SIDE INLET CATCH BASINS, SHALL BE CONSTRUCTED WITH PARTIAL STREET IMPROVEMENTS.



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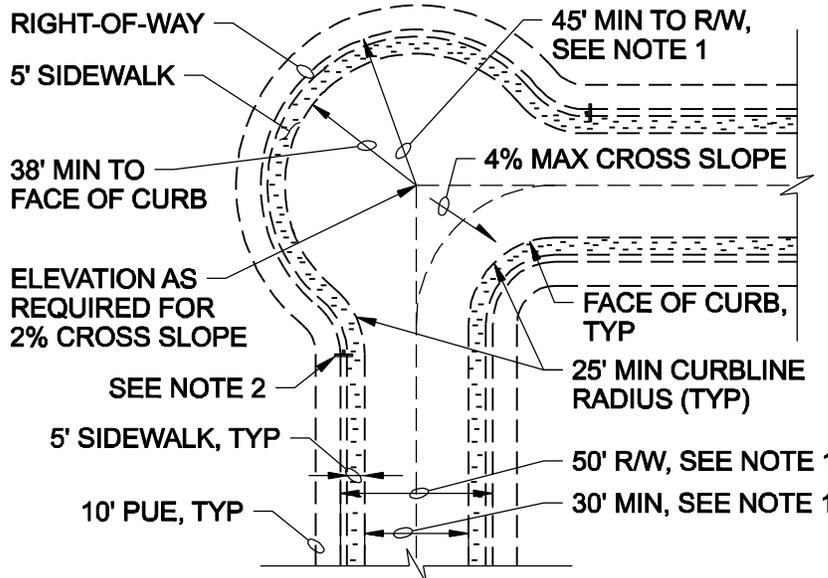
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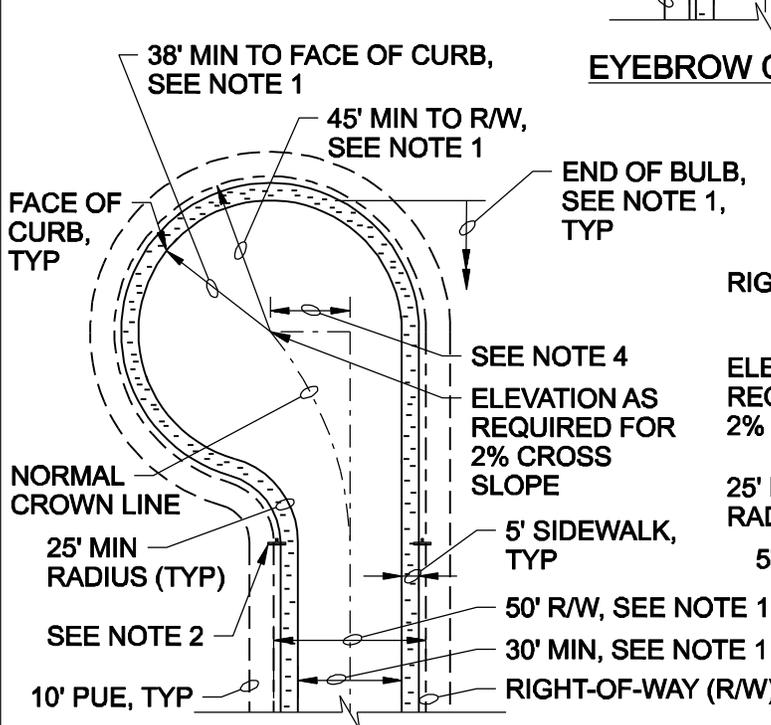
TYPICAL PARTIAL STREET SECTION

NO SCALE
 DIVISION
 STREET
 DRAWING NO.

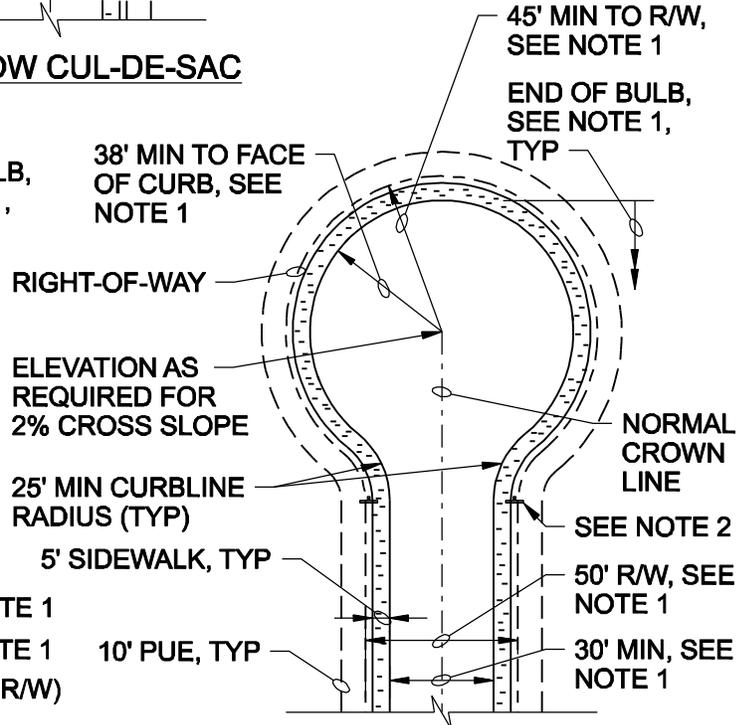
306



EYEBROW CUL-DE-SAC



OFFSET CUL-DE-SAC



STANDARD CUL-DE-SAC

NOTES:

1. WHEN CUL-DE-SACS ARE LESS THAN 200' TO END OF BULB (SHORT CUL-DE-SAC), STREET SECTION SHALL BE AS SHOWN. WHEN CUL-DE-SAC IS 200' OR LONGER, THE STREET SECTION SHALL BE 34' WIDE BETWEEN FACE OF CURBS AND LOCATED WITHIN A 60' RIGHT-OF-WAY (R/W). CUL-DE-SAC BULB RADIUS AND RIGHT-OF-WAY SHOWN IS FOR LOCAL RESIDENTIAL STREETS ONLY. ALL OTHER STREET CLASSIFICATIONS SHALL BE 45' MINIMUM BULB RADIUS WITHIN A 60' RIGHT-OF-WAY.
2. "NO PARKING BEYOND THIS POINT" SIGNS SHALL BE INSTALLED AT BOTH SIDES OF CUL-DE-SAC BULB ENTRANCE AS SHOWN.
3. A 2% MIN CROSS SLOPE IS REQUIRED FROM CENTER OF BULB TO GUTTER. MINIMUM GUTTER SLOPE SHALL BE 0.5%.
4. MAINTAIN NORMAL CROWN LINE TO CENTER OF CUL-DE-SAC BULB. OFFSET FROM STREET CENTERLINE TO CENTER OF BULB EQUALS THE BULB RADIUS MINUS ONE-HALF THE STREET WIDTH.



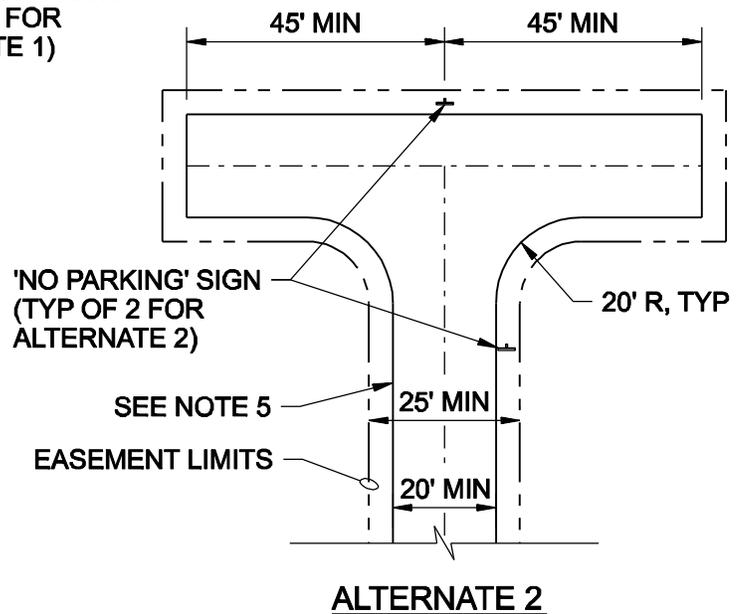
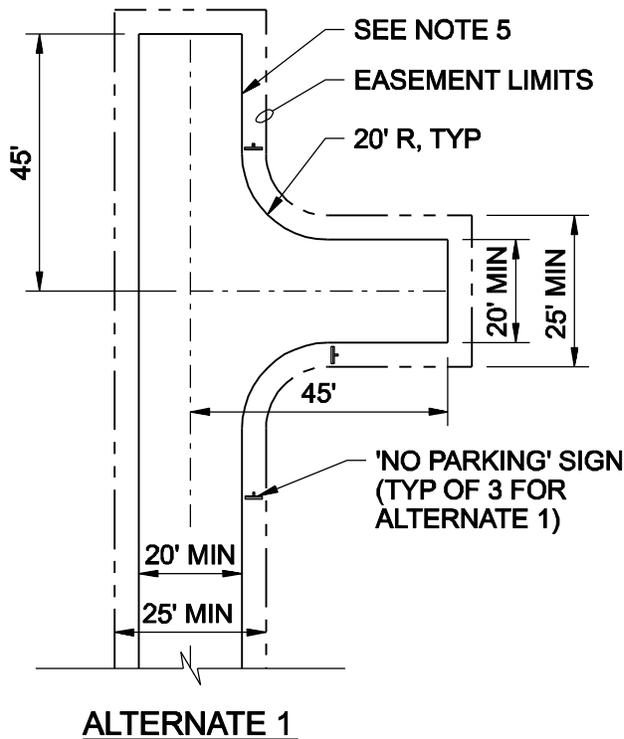
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TYPICAL CUL-DE-SAC

NO SCALE
DIVISION
STREET
DRAWING NO.
308



NOTES:

1. THE USE OF THIS HAMMER-HEAD TURNAROUND WILL BE LIMITED TO CERTAIN APPLICATIONS AND WILL REQUIRE APPROVAL OF THE CITY ENGINEER.
2. THE MAXIMUM LENGTH OF A PRIVATE STREET WITH THIS HAMMER-HEAD TURNAROUND SHALL BE 150 FEET TO THE END OF THE TURNAROUND. DEAD END PRIVATE STREETS IN EXCESS OF 150 FEET SHALL MEET THE CITY AND THE FIRE DISTRICT ACCESS, TURN-OUT, AND TURNAROUND STANDARDS.
3. "NO PARKING" SIGNS ARE REQUIRED WITHIN THE ACCESS EASEMENT LIMITS AND TURNAROUND AREA AS SHOWN.
4. A "PRIVATE STREET" SIGN SHALL BE POSTED AT THE ENTRANCE TO THE DEAD-END STREET.
5. PAVEMENT DIMENSIONS SHOWN ARE TO EDGE OF PAVEMENT OR FACE OF CURB.
6. FIRE HYDRANTS SHALL NOT BE LOCATED WITHIN PRIVATE STREETS HAVING A HAMMER-HEAD TURNAROUND, UNLESS SPECIFICALLY APPROVED BY THE CITY ENGINEER.
7. DESIGNS SHOWN ARE TYPICAL MINIMUM DESIGNS. ALTERNATE DESIGNS SHALL MEET THE APPROVAL OF THE CITY ENGINEER.



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STANDARD DRAWING TITLE

**HAMMER-HEAD
TURNAROUND**
(FOR PRIVATE STREETS ONLY)

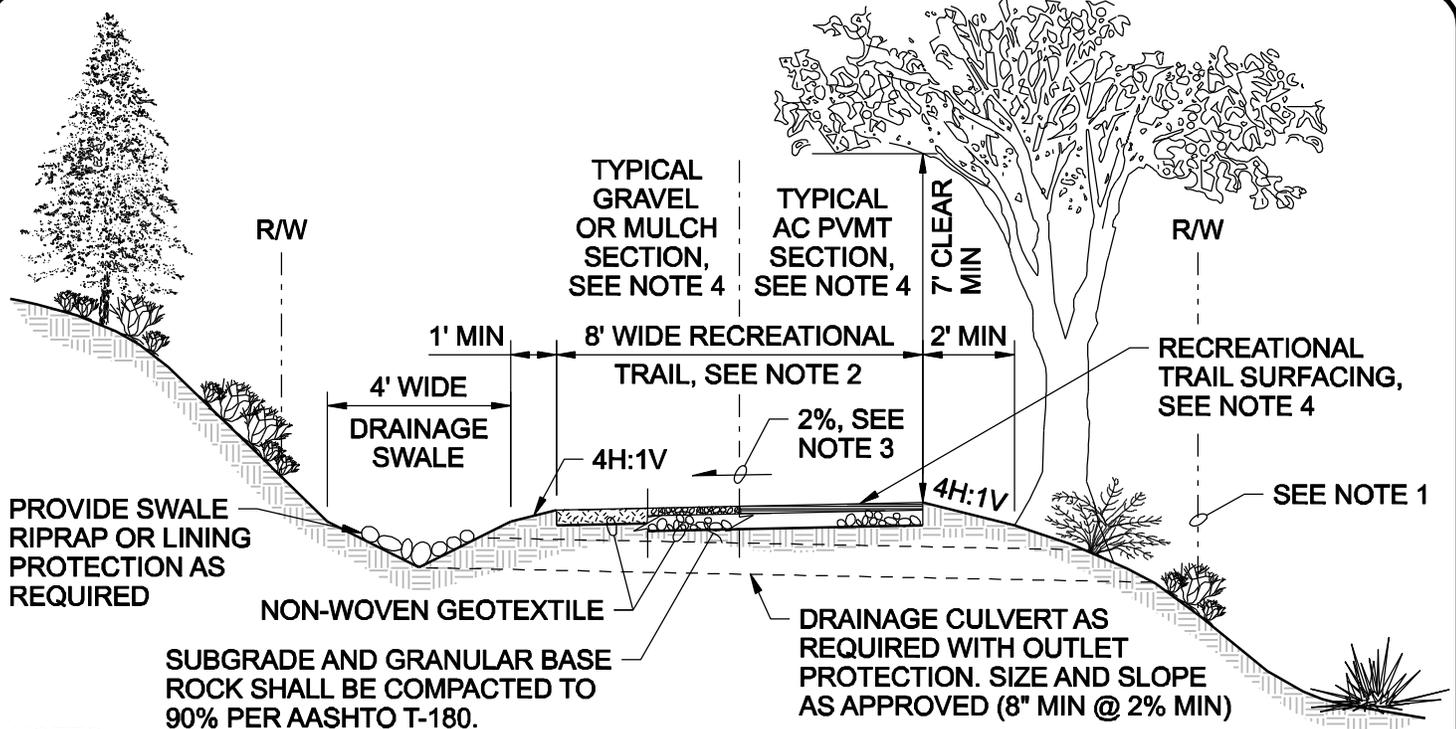
NO SCALE

DIVISION

STREET

DRAWING NO.

310



NOTES:

1. TRAIL RIGHT-OF-WAY OR EASEMENT WIDTH SHALL BE A MINIMUM OF 20 FEET OR AS REQUIRED BY THE CITY ENGINEER. TRAIL SHALL BE LOCATED A MINIMUM OF 20 FEET FROM THE TOP OF BANKS OF RIVERS, STREAMS, OR OTHER WATERWAYS TO PROTECT NATURAL RIPARIAN VEGETATION AND OTHER SENSITIVE AREAS. WHERE REQUIRED, TRAIL SHALL BE INTERRUPTED BY PERIODIC POINTS OF ACCESS TO PERMIT VIEWING OF THE WATERWAY FROM THE BANK.
2. TRAIL SHALL BE 8' WIDE AND SHALL BE FOR PEDESTRIAN USE ONLY. TRAIL WIDTH MAY BE REDUCED TO 4' WIDE MINIMUM IN AREAS OF STEEP OR DIFFICULT TERRAIN WHERE SPECIFICALLY APPROVED BY THE CITY ENGINEER. TRAIL SHALL HAVE 2' WIDE NATIVE SHOULDERS ON FILL SLOPES AND 1' WIDE NATIVE SHOULDERS WITH 4' WIDE DRAINAGE SWALES ON CUT SLOPES AS SHOWN.
3. TRAIL CROSS SLOPE SHALL BE 2%. IN AREAS OF STEEP OR DIFFICULT TERRAIN, A 5% CROSS SLOPE MAY BE ALLOWED WHERE APPROVED BY THE CITY ENGINEER. TRAILS THAT EXCEED AN 18 PERCENT LONGITUDINAL GRADE SHALL BE CONSTRUCTED WITH STAIRS AND HANDRAILS. STAIR DIMENSIONS SHALL BE THE TRAIL WIDTH BY 5 FEET LONG MINIMUM. THE USE OF SWITCHBACKS OR OTHER ALTERNATIVES TO STAIRS MAY BE USED WHERE SPECIFICALLY APPROVED BY THE CITY ENGINEER.
4. TRAIL SHALL BE CONSTRUCTED OF 2-INCHES OF CLASS "C" ASPHALT CONCRETE PAVEMENT OVER 4-INCHES OF COMPACTED 3/4"-0" GRANULAR BASE ROCK, UNLESS OTHERWISE APPROVED. TRAILS REQUIRED TO BLEND WITH THE SIDEWALK SYSTEM IN PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED OF PORTLAND CEMENT CONCRETE TO SIDEWALK STANDARDS. TRAILS REQUIRED TO BLEND WITHIN A NATURAL RURAL SETTING, AS DETERMINED BY THE CITY ENGINEER, SHALL BE CONSTRUCTED OF 2-INCHES OF COMPACTED 1/4"-0" CRUSHED AGGREGATE OVER 4-INCHES OF COMPACTED 3/4"-0" GRANULAR BASE ROCK. TRAILS DESIGNATED AS "RUSTIC" SHALL BE CONSTRUCTED OF 4-INCHES MINIMUM OF A CITY ENGINEER APPROVED BARK MULCH OR AN APPROVED UNPROCESSED MIX OF BARK AND WOOD FIBER (HOG FUEL). PAVEMENT OR OTHER APPROVED HARD SURFACING WILL BE REQUIRED IN LOCATIONS NEAR WETLANDS OR OTHER MARSHY AREAS.
5. EXISTING VEGETATION AND/OR OTHER OBSTRUCTIONS SHALL BE REMOVED COMPLETELY IN AREAS RECEIVING A HARD SURFACING, INCLUDING ALL STUMPS AND ROOTS. OTHER EXISTING VEGETATION SHALL BE REMOVED ONLY AS NECESSARY TO PERMIT THE INSTALLATION OF THE NEW TRAIL SYSTEM. AREAS BEYOND THE TRAIL CONSTRUCTION LIMITS SHALL NOT BE DISTURBED. ALL UNSUITABLE MATERIALS SHALL BE DISPOSED OF IN A MANNER AND LOCATION APPROVED BY THE CITY ENGINEER. ALL DISTURBED SURFACES NOT RECEIVING A HARD SURFACING SHALL BE SEEDED WITH AN APPROVED NON-INVASIVE NATIVE GRASS VARIETY.

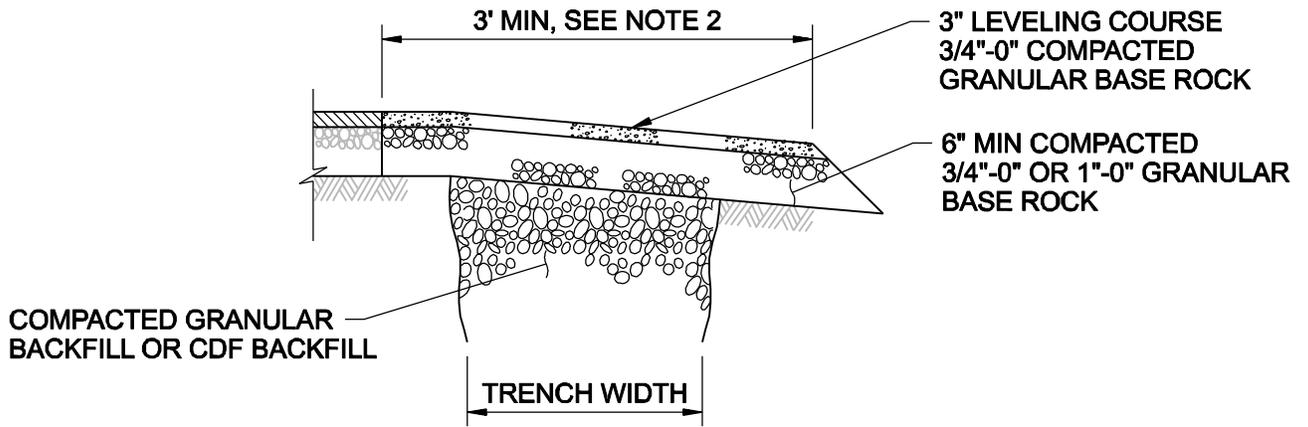


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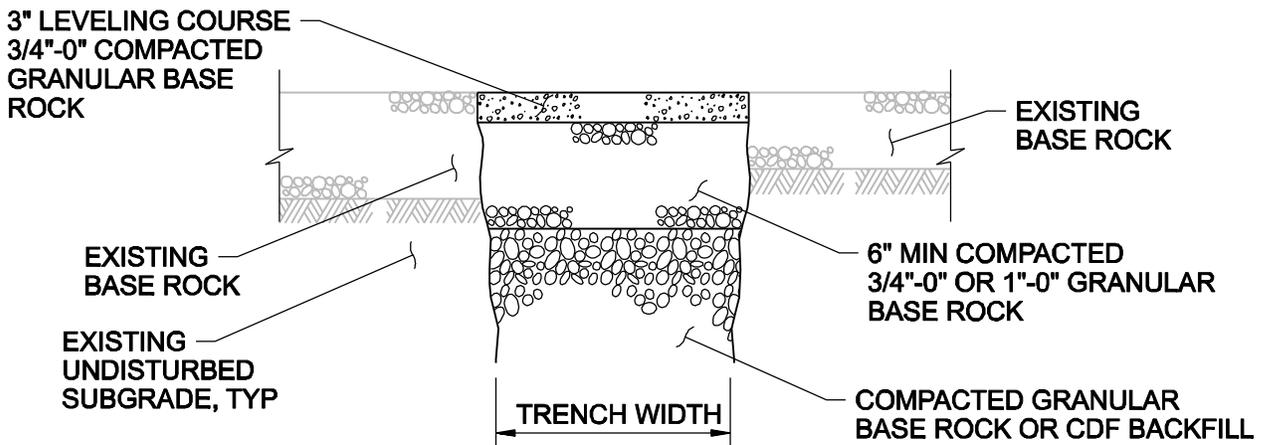
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RECREATIONAL TRAIL

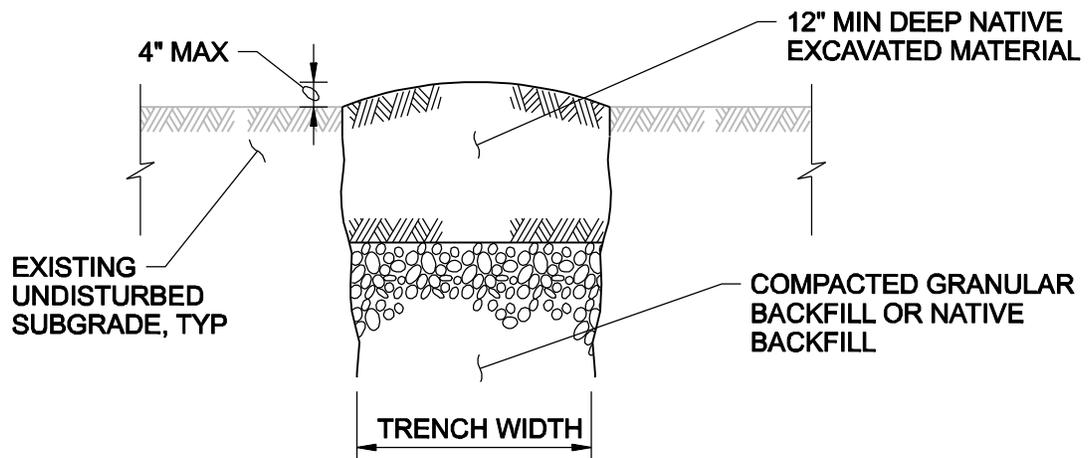
NO SCALE
DIVISION
STREET
DRAWING NO.
314



GRAVEL SURFACING FOR SHOULDER



GRAVEL SURFACING



NATIVE SURFACING

NOTES:

1. SEE THE STANDARD CONSTRUCTION SPECIFICATIONS FOR COMPACTION REQUIREMENTS.
2. SHOULDER SHALL BE RESTORED TO 3' MINIMUM WHERE ANY PORTION OF THE TRENCH EXCAVATION IS WITHIN THE SHOULDER AREA.



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**GRAVEL AND NATIVE
SURFACE
RESTORATION**

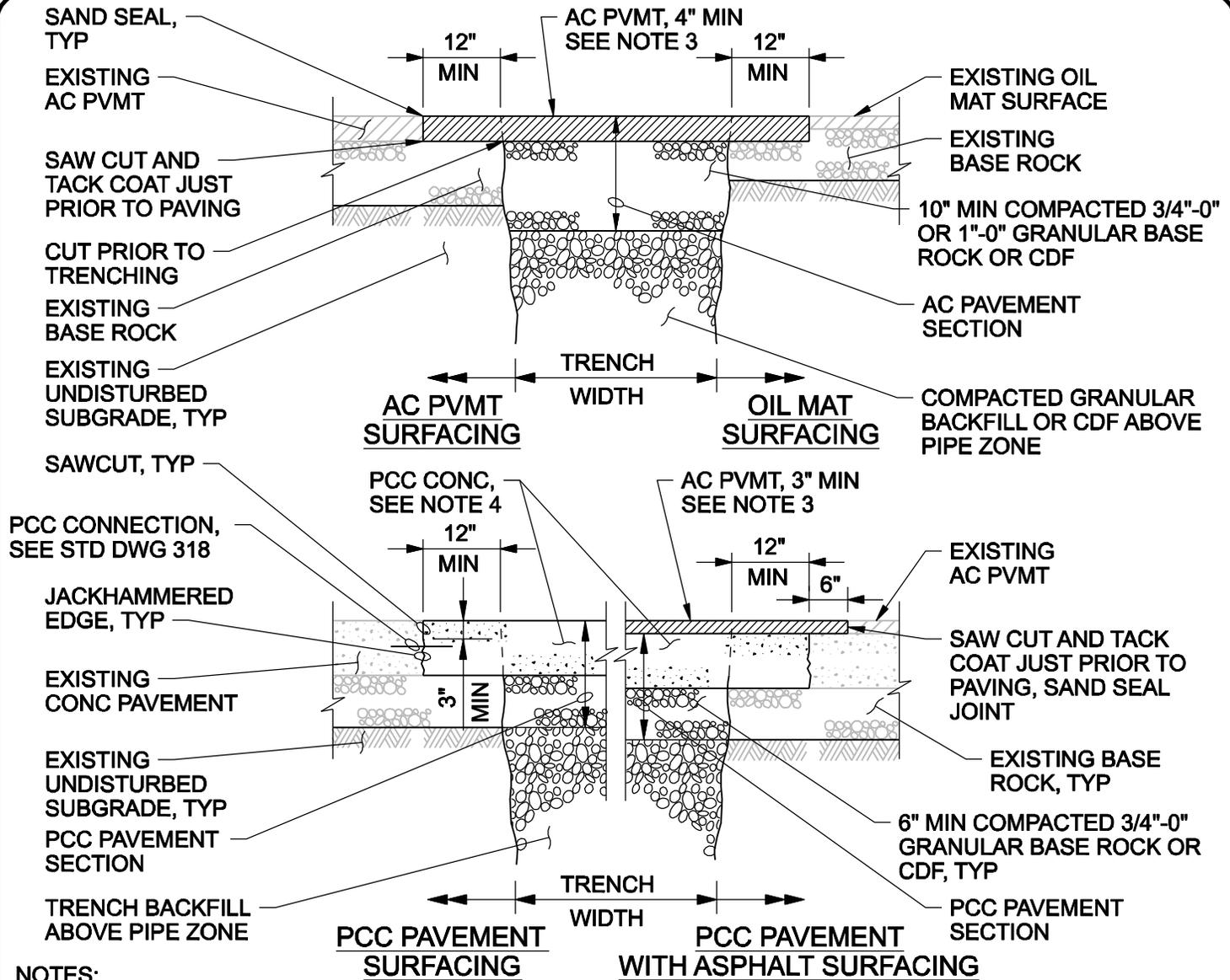
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DIVISION

STREET

DRAWING NO.

316



NOTES:

1. IF LOCATION OF TRENCH SAWCUT IS WITHIN 3' OF AN EXISTING JOINT OR EDGE, REPLACE THE ENTIRE ASPHALT OR PORTLAND CEMENT CONCRETE PAVEMENT SECTION TO THE EXISTING JOINT OR EDGE.
2. ALL EXISTING AC AND PCC PAVEMENT SHALL BE SAWCUT JUST PRIOR TO PAVING. ALL CUT EDGES SHALL BE FREE OF LOOSE MATERIAL. SEE STANDARD DRAWING 304 FOR TYPICAL AC PVMT SECTION.
3. AC PAVEMENT SHALL BE LEVEL 2, CLASS "C" MIX COMPACTED IN TWO EQUAL LIFTS TO 91% PER AASHTO T-209. PAVEMENT THICKNESS SHALL MATCH EXISTING OR 4" MIN, WHICHEVER IS GREATER. ALL EDGES SHALL BE TACK COATED PRIOR TO PAVING AND SAND SEALED AFTER COMPLETION.
4. PCC PAVEMENT SHALL BE 5,000 PSI MIN AND SHALL MATCH EXISTING THICKNESS OR 7" MIN FOR LOCAL STREETS, 8" MIN FOR COLLECTOR STREETS, OR 9" MIN FOR ARTERIAL STREETS, WHICHEVER IS GREATER. CONSTRUCT JOINTS ACROSS NEW CONCRETE TO MATCH EXISTING JOINT TYPES AND LOCATIONS. SEE STANDARD DRAWING 304 FOR TYPICAL PCC PAVEMENT SECTION.
5. GRANULAR BASE ROCK SHALL BE 1"-0" COMPACTED TO A MIN 95% PER AASHTO T-180. THICKNESS SHALL MATCH EXISTING OR BE MINIMUM SHOWN, WHICHEVER IS GREATER.
6. A FULL WIDTH STREET OVERLAY OR GRIND/INLAY MAY BE REQUIRED DEPENDING UPON THE EXTENT OF NEW AND EXISTING PAVEMENT CUTS.

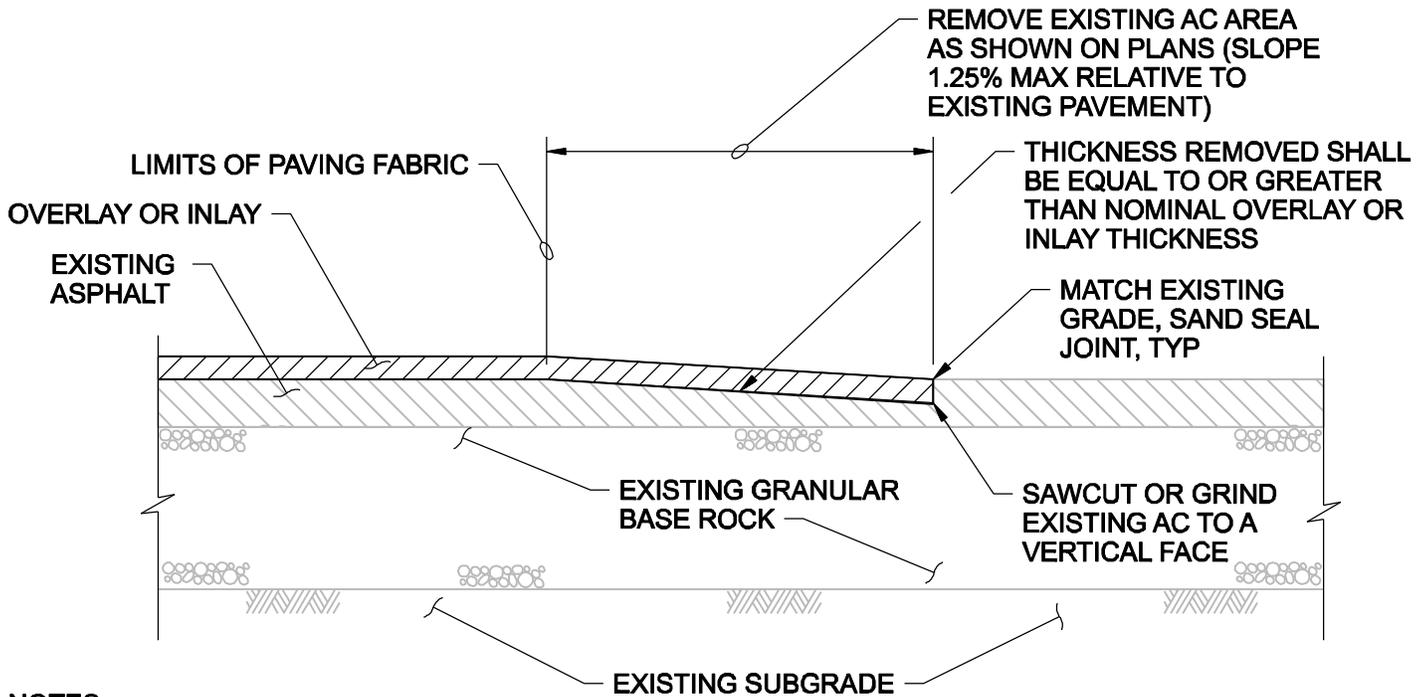


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STANDARD DRAWING TITLE
PAVEMENT SURFACE RESTORATION

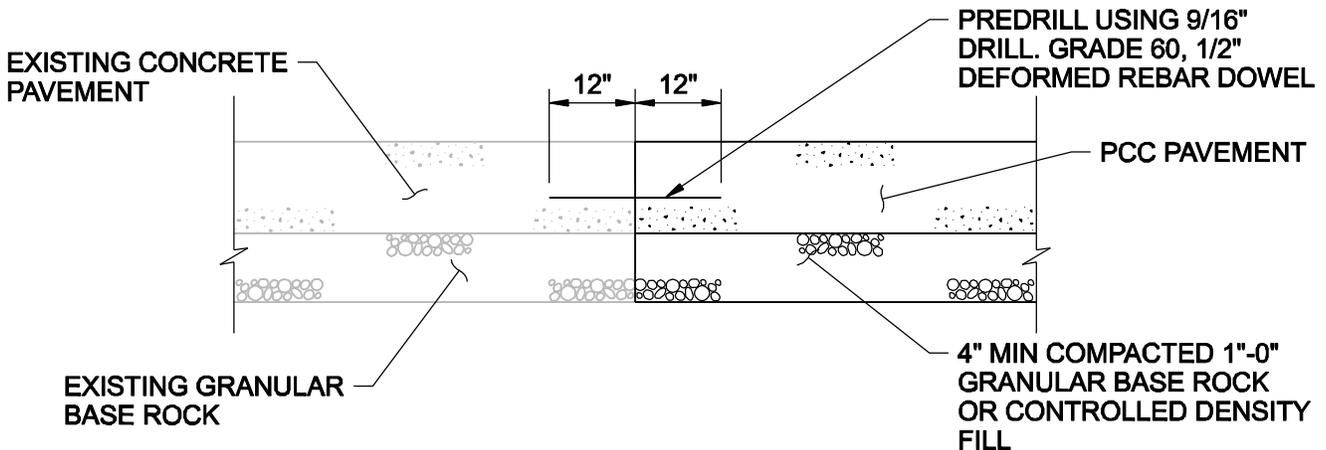
NO SCALE
DIVISION
STREET
DRAWING NO.
318



NOTES:

1. ASPHALT CONCRETE PAVEMENT SURFACES SHALL BE PROPERLY PREPARED AND TACK COATED PRIOR TO PLACEMENT OF ASPHALT OVERLAY OR GRIND INLAY.
2. ASPHALT CONCRETE PAVEMENT SHALL BE COMPACTED TO 91% PER AASHTO T-209.

AC PAVEMENT NOTCHING DETAIL



NOTES:

1. EXISTING PORTLAND CEMENT CONCRETE PAVEMENT SHALL BE DRILLED USING APPROPRIATE MASONRY DRILL BIT.
2. DOWELS SHALL BE PLACED ALONG THE CENTERLINE OF THE PCC PAVEMENT AT 2' ON CENTER.

PCC PAVEMENT CONNECTION

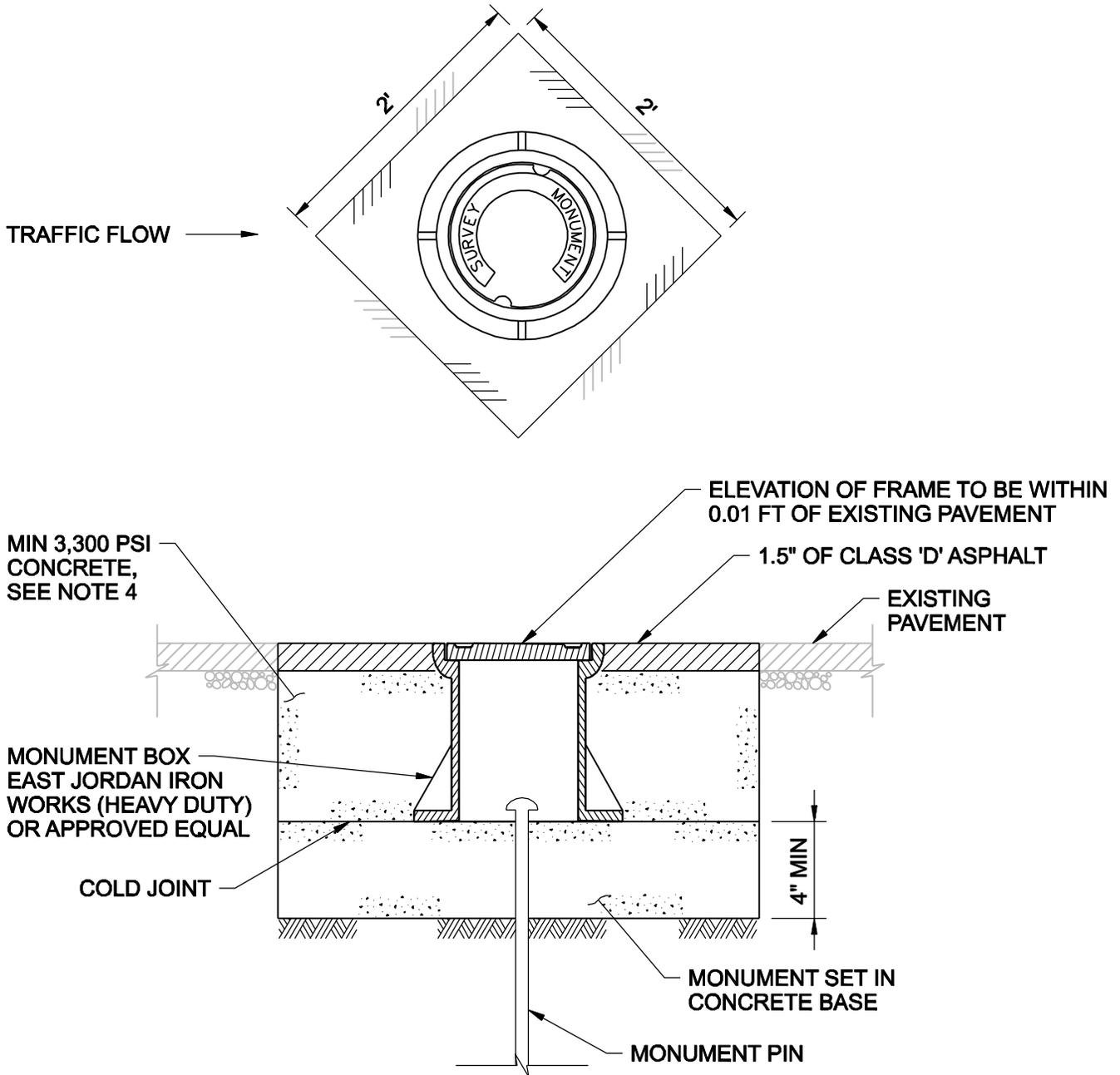


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STANDARD DRAWING TITLE
PAVEMENT CONNECTION DETAILS

NO SCALE
DIVISION
STREET
DRAWING NO.
320



NOTES:

1. A COLD JOINT SHALL BE CONSTRUCTED BETWEEN THE MONUMENT PIN BASE AND THE CONCRETE THAT IS PLACED DURING SURFACE RESTORATION.
2. THE MONUMENT BOX SHALL NOT BE CAST INTO THE MONUMENT PIN BASE.
3. FINISH ELEVATION OF SURVEY MONUMENT BOXES SHALL BE WITHIN 0.01 FOOT OF THE ADJACENT STREET FINISH GRADE. IF THE DIFFERENCE IN ELEVATION EXCEEDS 0.01 FOOT, THE MONUMENT BOX ASSEMBLY SHALL BE ADJUSTED AS SPECIFIED IN NOTE 4.
4. FOR SURVEY MONUMENT INSTALLATION IN EXISTING STREETS NOT SCHEDULED FOR NEW SURFACING, A 2' x 2' SQUARE OF ASPHALT SHALL BE REMOVED AND THE ENTIRE AREA EXCAVATED TO A DEPTH OF APPROXIMATELY 9.5" BELOW FINISH GRADE OF THE STREET. CONCRETE SHALL BE PLACED TO A DEPTH OF APPROXIMATELY 8" WITHIN THE ENTIRE CUT OUT AREA IN TWO SEPARATE POURS AS SHOWN. THE CONCRETE SHALL BE COVERED WITH CLASS "D" ASPHALT WITH A MINIMUM DEPTH OF 1.5".



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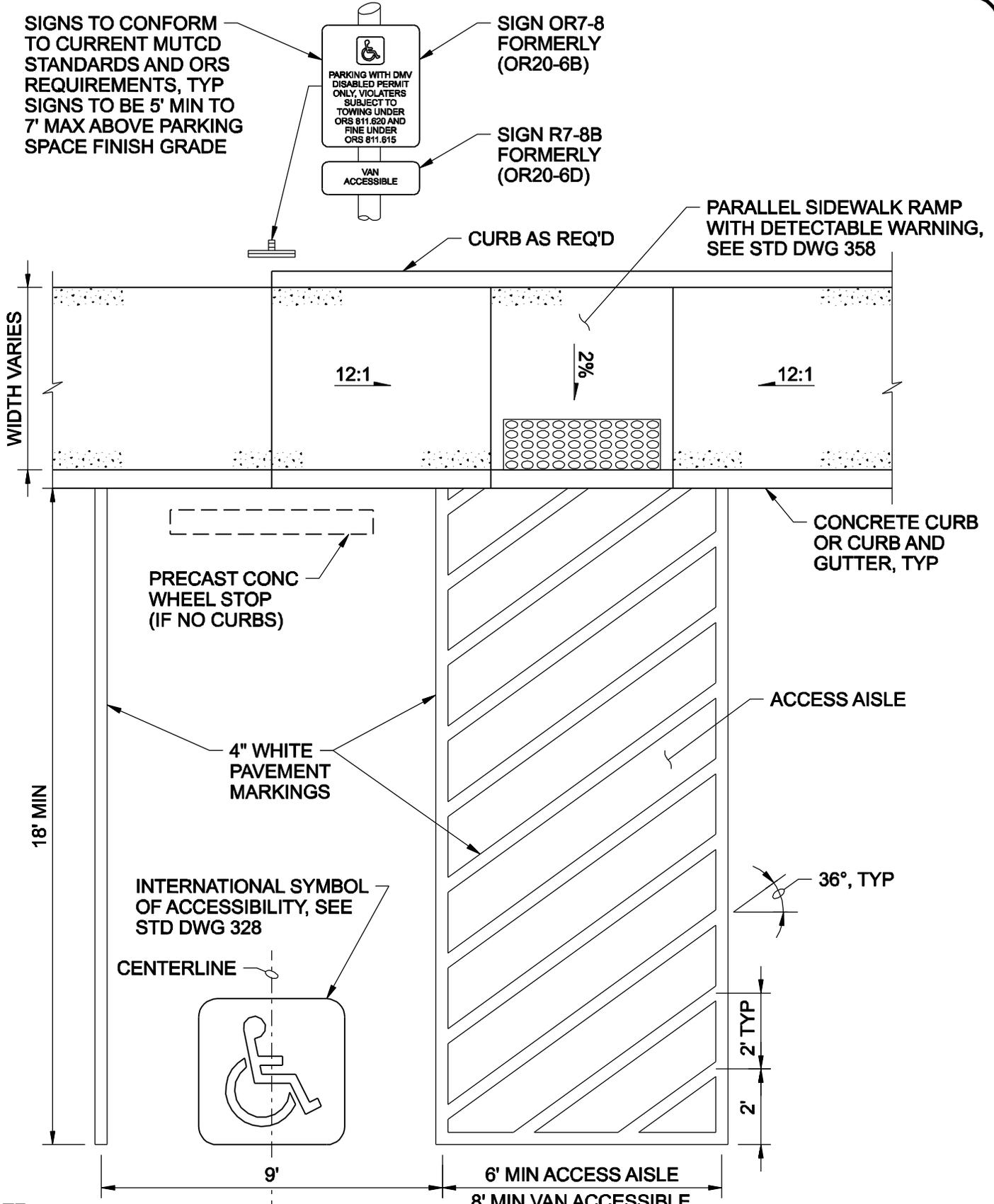
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STANDARD DRAWING TITLE

**SURVEY
MONUMENT BOX**

NO SCALE
DIVISION
STREET
DRAWING NO.

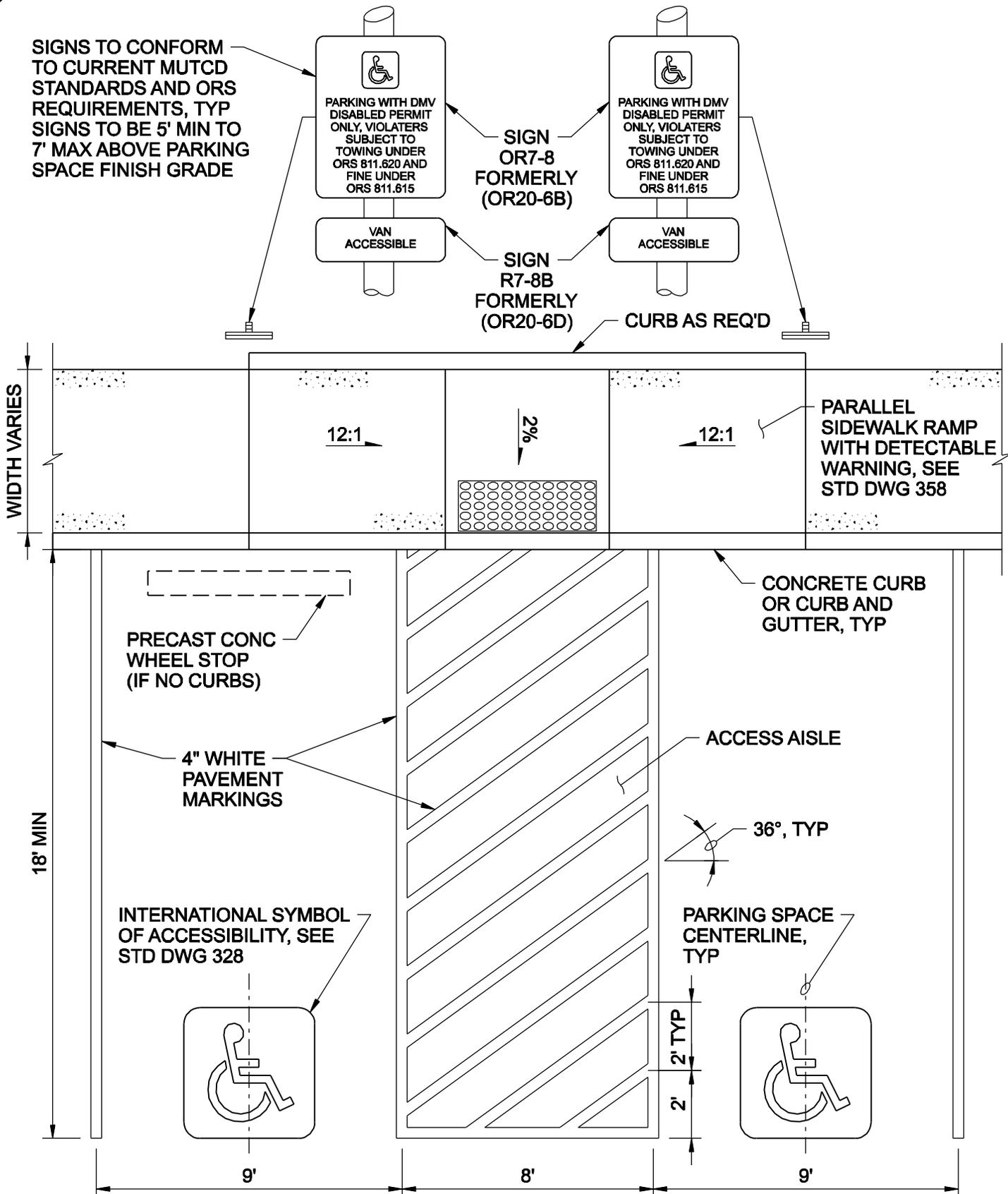
322



NOTE:

1. MAXIMUM 2% SLOPE WITHIN ACCESSIBLE PARKING SPACE OR ACCESS AISLE.

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	<p>REVISIONS</p>		<p>DIVISION</p>
			<p>STREET</p>
			<p>DRAWING NO.</p> <p>324</p>



NOTE:

1. MAXIMUM 2% SLOPE WITHIN ACCESSIBLE PARKING SPACES OR ACCESS AISLE.



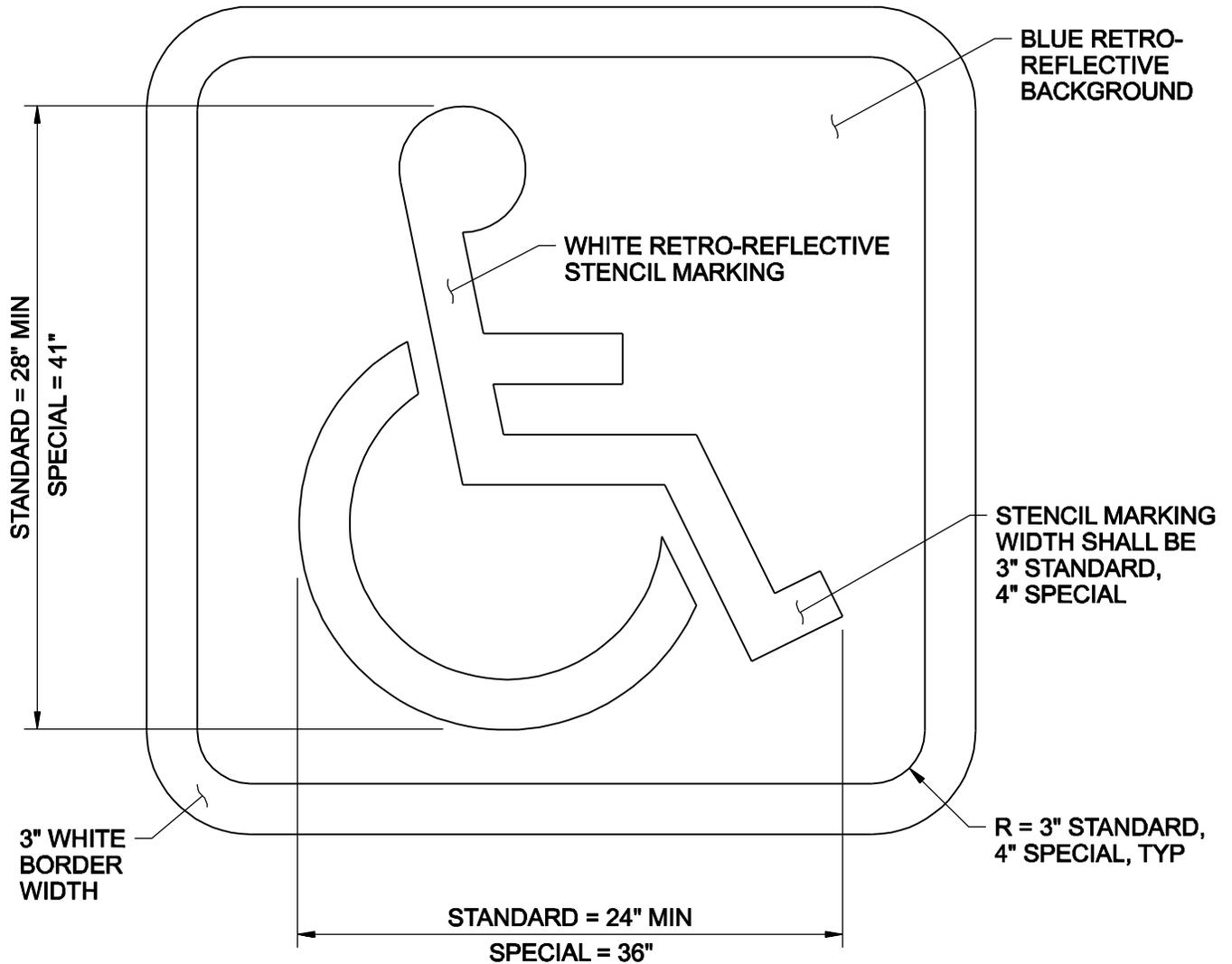
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DOUBLE ACCESSIBLE PARKING SPACE

NO SCALE
DIVISION
STREET
DRAWING NO.
326



NOTES:

1. PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH MUTCD 3B-19 AND SHALL BE USED TO DESIGNATE ANY ACCESSIBLE PARKING AREA RESERVED FOR VEHICLES WITH DEPARTMENT OF MOTOR VEHICLE PERMITS. THE STANDARD SYMBOL PAVEMENT MARKING SIZE AND WIDTHS SHALL BE USED, UNLESS OTHERWISE DIRECTED BY THE CITY ENGINEER.
2. PAVEMENT MARKING BACKGROUND SHALL BE BLUE RETRO-REFLECTIVE. INTERIOR STENCIL AND EXTERIOR BORDER SHALL BE WHITE RETRO-REFLECTIVE, 3-INCH WIDE FOR STANDARD SYMBOLS AND 4-INCH WIDE FOR SPECIAL SYMBOLS.



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**INTERNATIONAL
SYMBOL OF
ACCESSIBILITY**

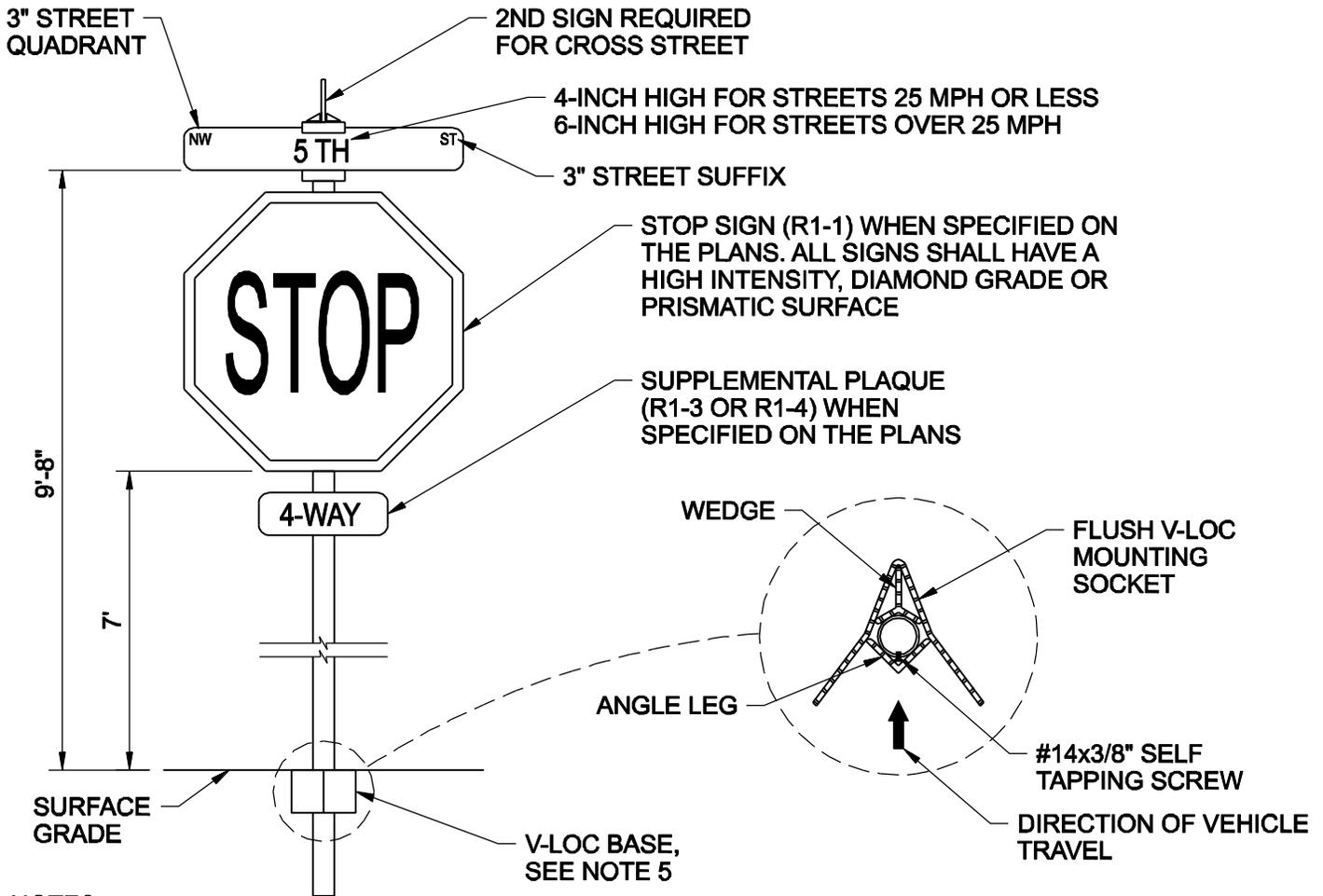
NO SCALE

DIVISION

STREET

DRAWING NO.

328



NOTES:

1. ALL SIGNS SHALL CONFORM TO THE CURRENT VERSION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE CURRENT OREGON SUPPLEMENT TO THE MUTCD.
2. STREET NAME SIGNS SHALL BE 6"x30", 6"x36", OR 9"x42" (FOR SIGNS WITH 6" LETTERS) EXTRUDED ALUMINUM, DOUBLE-FACED WITH ENGINEERS GRADE WHITE ON GREEN REFLECTIVE SHEETING. PROVIDE EACH POLE WITH A SECOND SIGN FOR A CROSSING STREET.
3. STOP SIGNS SHALL BE 0.080-INCH THICK ANODIZED ALUMINUM. UNLESS SPECIFIED OTHERWISE, REGULATORY AND WARNING SIGNS SHALL BE A MINIMUM SIZE OF 30-INCHES.
4. SIGNS SHALL BE MOUNTED ON THE POST WITH HAWKINS, SINGLE CLAMP ON U-BRACKETS, WITH HEX HEAD SCREWS. GALVANIZED PRESS-ON PIPE CAPS SHALL BE INSTALLED ON THE TOP OF THE SIGN POST WHEN NO STREET SIGNS ARE PRESENT.
5. POLE BASE SHALL BE V-LOC ANCHOR SYSTEM MODEL 23-VR3 (SOIL APPLICATION) OR 23-VR1 (CONCRETE APPLICATION), OR APPROVED EQUAL. WHEN POLE IS PLACED IN CONCRETE, CONCRETE SHALL NOT COVER THE V-LOC WEDGE UNLESS OTHERWISE DIRECTED BY THE CITY ENGINEER.
6. POST SHALL BE 2 3/8" OUTSIDE DIAMETER GALVANIZED STEEL PIPE WITH 0.095" WALL THICKNESS.
7. ALL STREET SIGN LETTERS SHALL BE UPPER CASE, HIGHWAY "C" FONT.
8. DISTANCE FROM THE FACE OF CURB TO THE EDGE OF THE STREET NAME SIGN SHALL BE 2' MIN.
9. FINAL SIGN LOCATIONS SHALL BE AS APPROVED BY THE CITY ENGINEER.



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STANDARD DRAWING TITLE

TYPICAL STREET
NAME AND
STOP SIGN

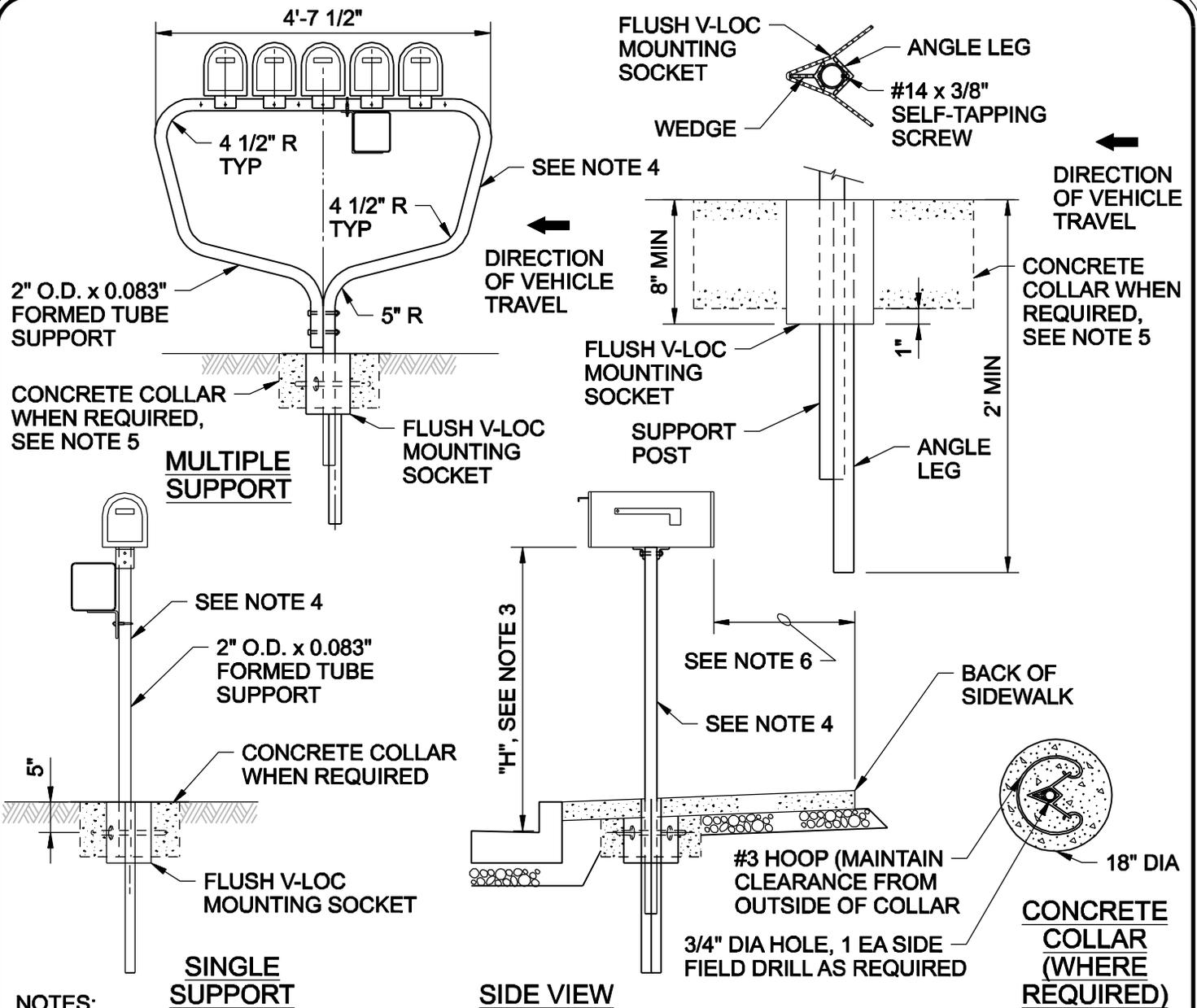
NO SCALE

DIVISION

STREET

DRAWING NO.

330



NOTES:

1. CONTACT THE USPS FOR REQUIREMENTS REGARDING LOCATIONS OF ALL MAILBOXES AND CLUSTER MAILBOXES. CLUSTER MAILBOX LOCATIONS SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH CHAPTER 11, SECTION 1111 OF THE OREGON STRUCTURAL SPECIALTY CODE (OSSC).
2. FACE OF MAILBOX SHALL BE 6" TO 12" FROM FACE OF CURB. ON STREETS WITHOUT CURBS, THE FRONT OF THE MAILBOX SHALL BE LOCATED AT THE OUTSIDE EDGE OF THE SHOULDER.
3. H = 50" MIN, 52" MAX FOR IMPROVED STREETS AND 36" MIN, 38" MAX FOR UNIMPROVED STREETS, UNLESS OTHERWISE DIRECTED BY THE USPS.
4. SUPPORTS SHALL BE MODEL 20-S (SINGLE), 20-D (DOUBLE) OR 20-M (MULTIPLE) MAILBOX SUPPORT KITS WITH V-LOC ANCHOR SYSTEM MODEL 23-VRS (SOIL APPLICATION) AND 23-VR1 (CONCRETE APPLICATION), AS MANUFACTURED BY TRAFFIC AND PARKING CONTROL COMPANY, INC OR APPROVED EQUAL.
5. CONCRETE SUPPORT COLLARS SHALL BE PROVIDED WHEN DIRECTED BY THE CITY ENGINEER.
6. SEE STANDARD DRAWING 342 FOR REQUIREMENTS RELATIVE TO CONSTRUCTION OF SIDEWALKS AROUND OBSTACLES.



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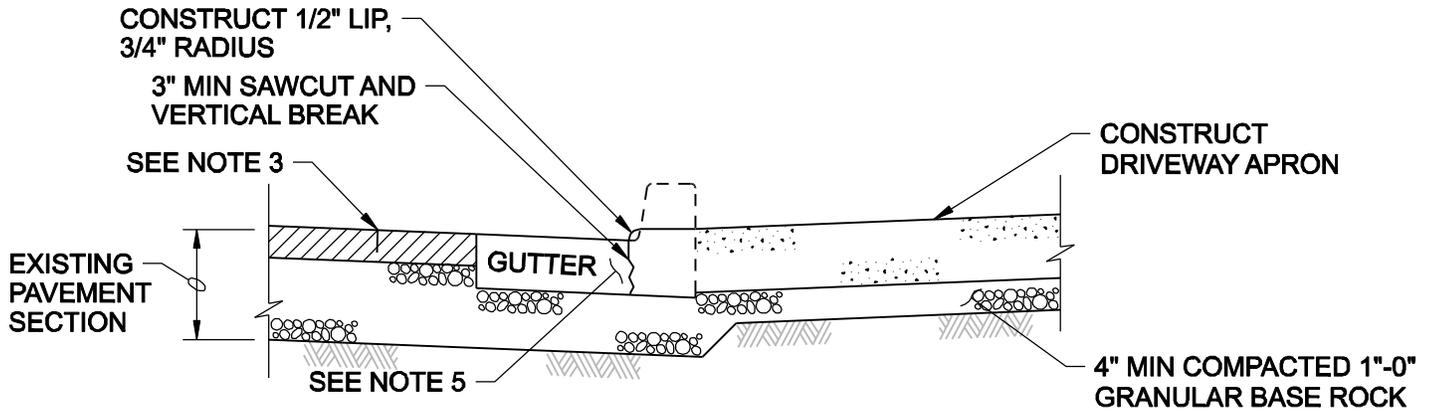
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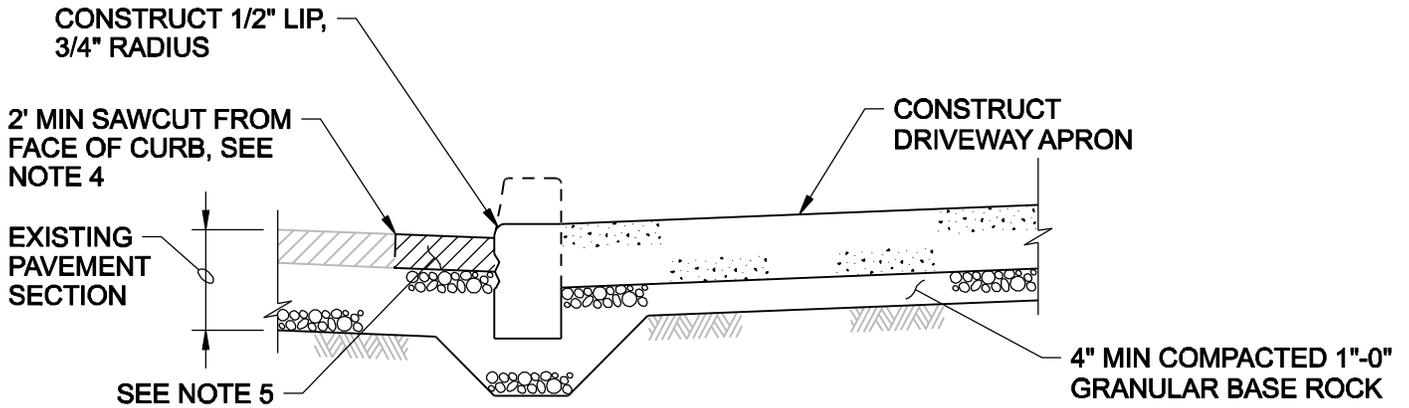
MAILBOX LOCATION

NO SCALE
 DIVISION
 STREET
 DRAWING NO.

332



CURB AND GUTTER



STANDARD CURB

NOTES:

1. SAWCUT MADE THROUGH GUTTER SHALL BE AS CLOSE TO CURB FACE AS POSSIBLE.
2. COMPLETE CURB AND GUTTER SHALL NOT BE REMOVED UNLESS DIRECTED BY THE CITY ENGINEER.
3. WHEN ENTIRE GUTTER IS TO BE REMOVED, THE EXISTING ASPHALT CONCRETE PAVEMENT SHALL BE SAWCUT BACK 2' FROM GUTTER AND REMOVED AND REPLACED WITH 4" MIN THICK CLASS "C" ASPHALT CONCRETE PAVEMENT. A 6" MONOLITHIC CONCRETE BENCH SHALL BE CONSTRUCTED WITH THE NEW GUTTER TO PROVIDE SUPPORT UNDER PAVEMENT.
4. WHEN STRAIGHT CURBS ARE REMOVED, A MINIMUM OF 2' OF ASPHALT CONCRETE PAVEMENT FROM THE FACE OF CURB SHALL BE REMOVED AND REPLACED WITH 4" MIN THICK CLASS "C" ASPHALT CONCRETE PAVEMENT.
5. ALL CONCRETE JOINTS SHALL BE SEALED AFTER CONCRETE HAS CURED AND ALL ASPHALT JOINTS SHALL BE SAND SEALED AFTER ASPHALT HAS BEEN PLACED. SEE THE STANDARD CONSTRUCTION SPECIFICATIONS.



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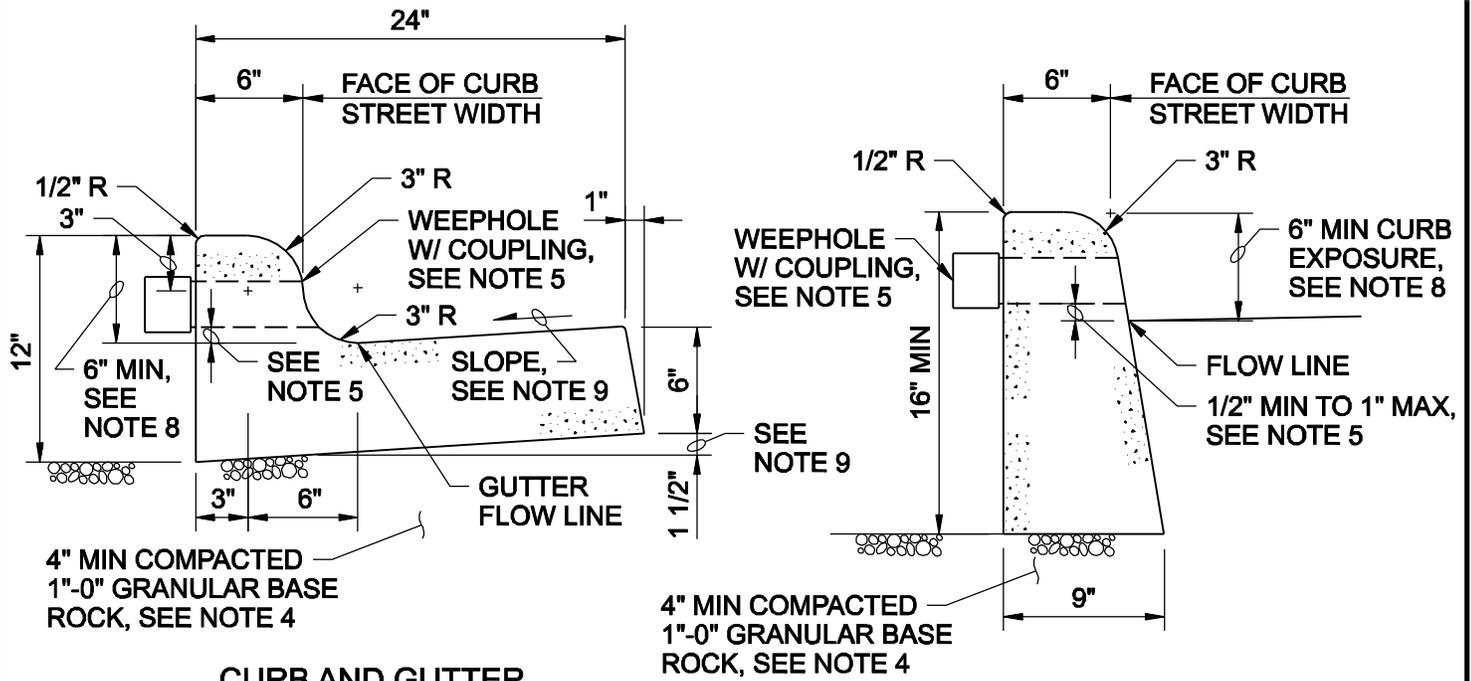
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STANDARD DRAWING TITLE

CURB KNOCKOUT FOR DRIVEWAYS

NO SCALE
 DIVISION
 STREET
 DRAWING NO.

334



CURB AND GUTTER

STRAIGHT CURB

NOTES:

1. CONCRETE TO HAVE COMPRESSIVE STRENGTH OF 3,300 PSI AT 28 DAYS, WITH LIGHT BROOM FINISH.
2. CONTRACTION JOINTS SHALL BE PLACED AT:
 - A SPACING OF NOT MORE THAN 15 FEET.
 - A DEPTH OF JOINT OF AT LEAST 1-1/2 INCHES.
 - ABOVE DRAINAGE WEEPHOLES.
3. EXPANSION JOINT MATERIAL TO BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF 1/2-INCH. EXPANSION JOINT TO BE PROVIDED AT EACH:
 - POINT OF TANGENCY.
 - COLD JOINT.
 - SIDES OF INLET STRUCTURES.
 - SIDES OF DRIVEWAYS.
 - LOCATIONS NECESSARY TO LIMIT SPACING TO 200'.
4. GRANULAR BASE ROCK SHALL BE 1"-0" COMPACTED TO 95% PER AASHTO T-180. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURE OR 4-INCHES, WHICHEVER IS GREATER, AND SHALL EXTEND A MINIMUM OF 12-INCHES BEHIND THE CURB.
5. DRAINAGE WEEPHOLES SHALL BE:
 - 3-INCH DIAMETER SCHEDULE 40 PVC PIPE, WITH COUPLING AS SHOWN.
 - CENTERED WITH CONTRACTION JOINTS.
 - CORE DRILLED THROUGH EXISTING CURBS FOR DRAINAGE ACCESS, 1/2" MINIMUM TO 1" MAXIMUM ABOVE FLOW LINE AS SHOWN. GROUT AROUND PIPE WITH APPROVED NON-SHRINK GROUT.
6. CURBS AND GUTTERS SHOWN MAY BE USED WITH EITHER AC OR PCC PAVEMENTS.
7. TRANSITION FROM ONE CURB TYPE TO ANOTHER WILL BE AS INDICATED ON THE PLANS AS NECESSARY.
8. CURB EXPOSURE FOR ARTERIAL STREETS SHALL BE 7-INCHES.
9. GUTTER SLOPES SHALL BE AT A NORMAL 8.33% AS SHOWN. A 5.0% MAXIMUM GUTTER SLOPE IS REQUIRED AT ALL ADA ACCESS RAMPS. GUTTER SLOPE TRANSITIONS SHALL BE 3' MINIMUM, OR AS INDICATED ON THE PLANS.



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REVISOR: REVISED 05/17

STANDARD DRAWING TITLE

**PCC CURB AND GUTTER
AND STRAIGHT CURBS**

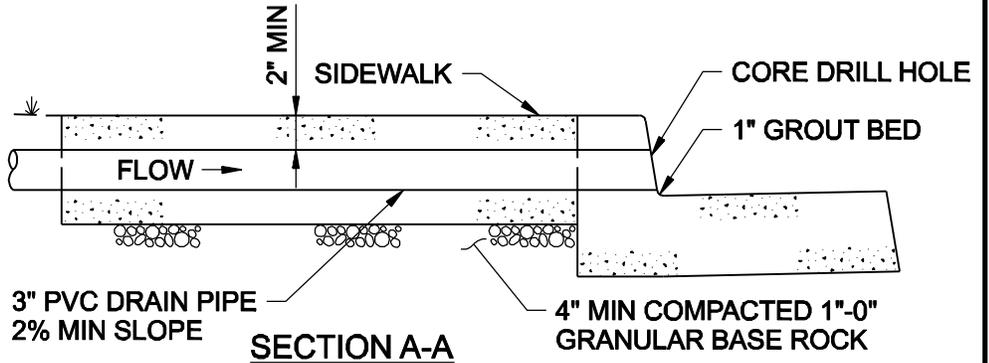
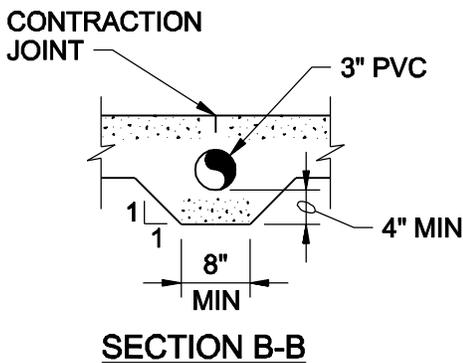
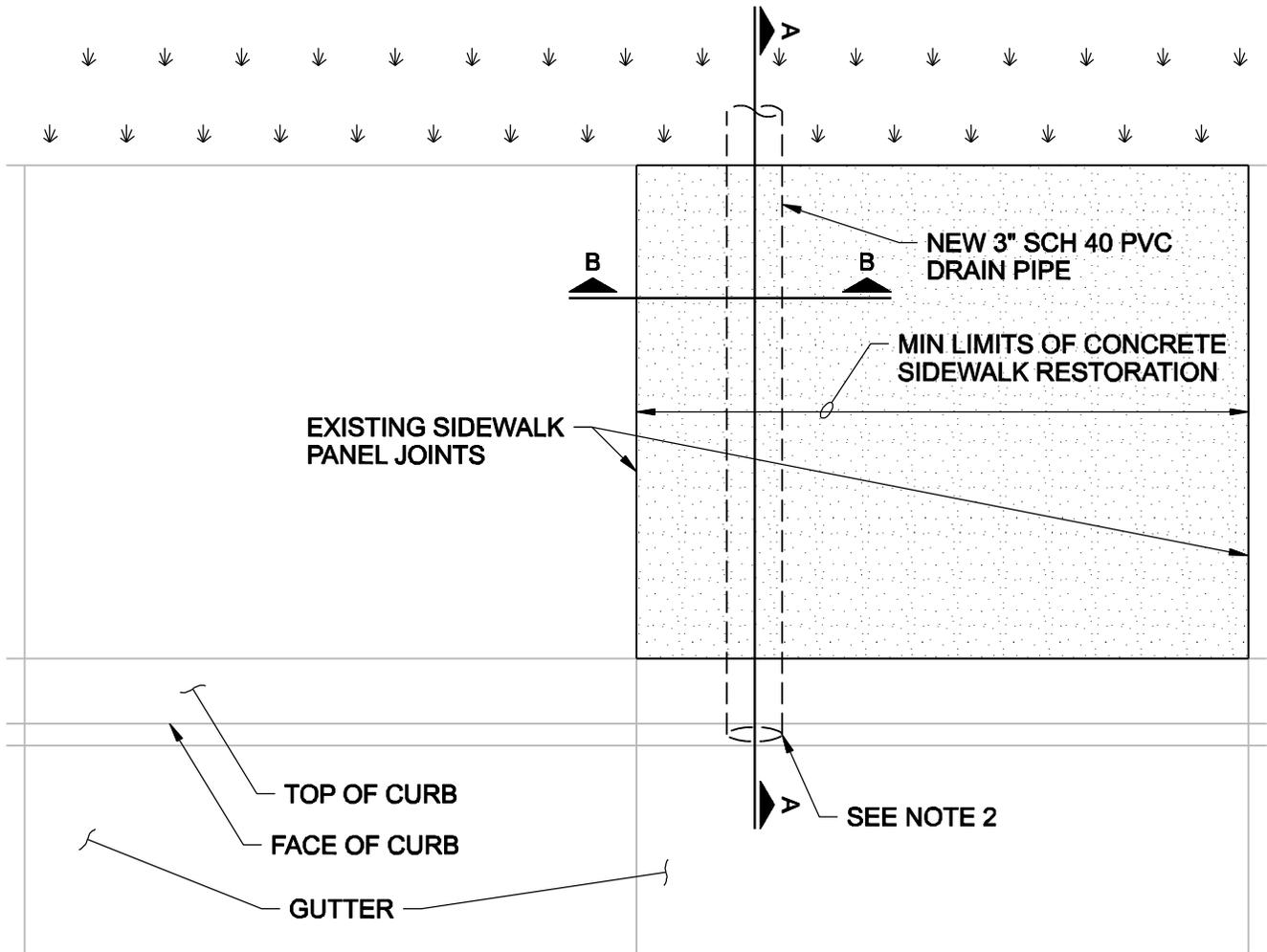
NO SCALE

DIVISION

STREET

DRAWING NO.

336



NOTES:

1. SAWCUT SIDEWALK TO FULL DEPTH. MINIMUM LIMIT OF CONCRETE REMOVAL AND REPLACEMENT IS ENTIRE AREA BETWEEN PANEL DIVISION MARKS.
2. CURB SHALL BE CORE DRILLED.
3. MATCH THICKNESS OF EXISTING CURB AND SIDEWALK.
4. FINISH SIDEWALK AND CURB (TOP AND FACE) FLUSH WITH EXISTING ADJACENT LINE AND GRADE.
5. THE MINIMUM 28-DAY COMPRESSIVE STRENGTH OF ALL CONCRETE SHALL BE 3,300 PSI.



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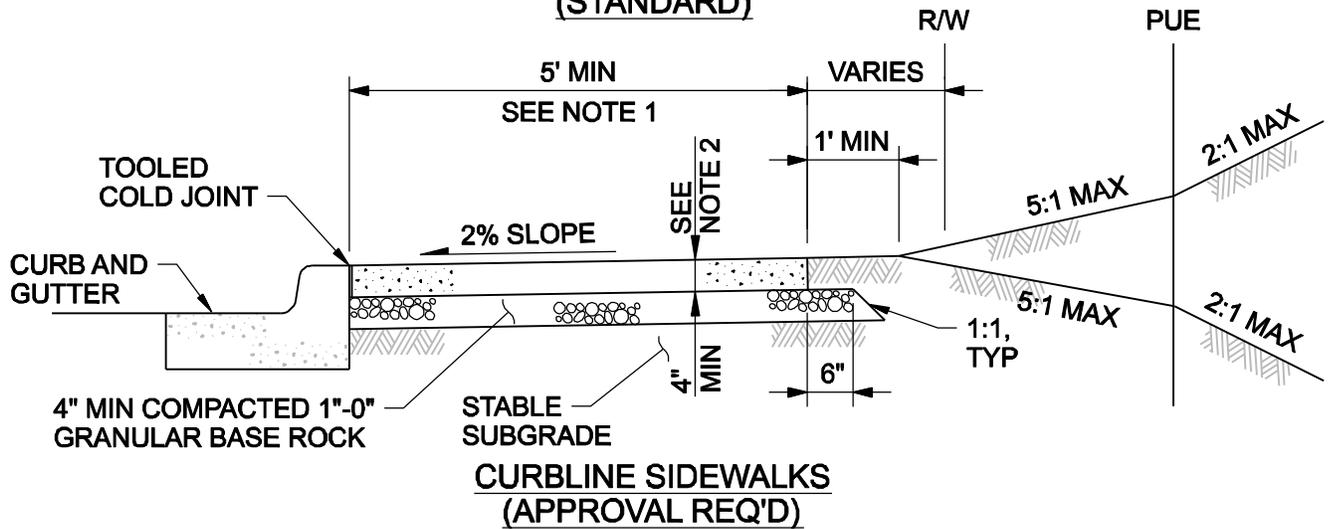
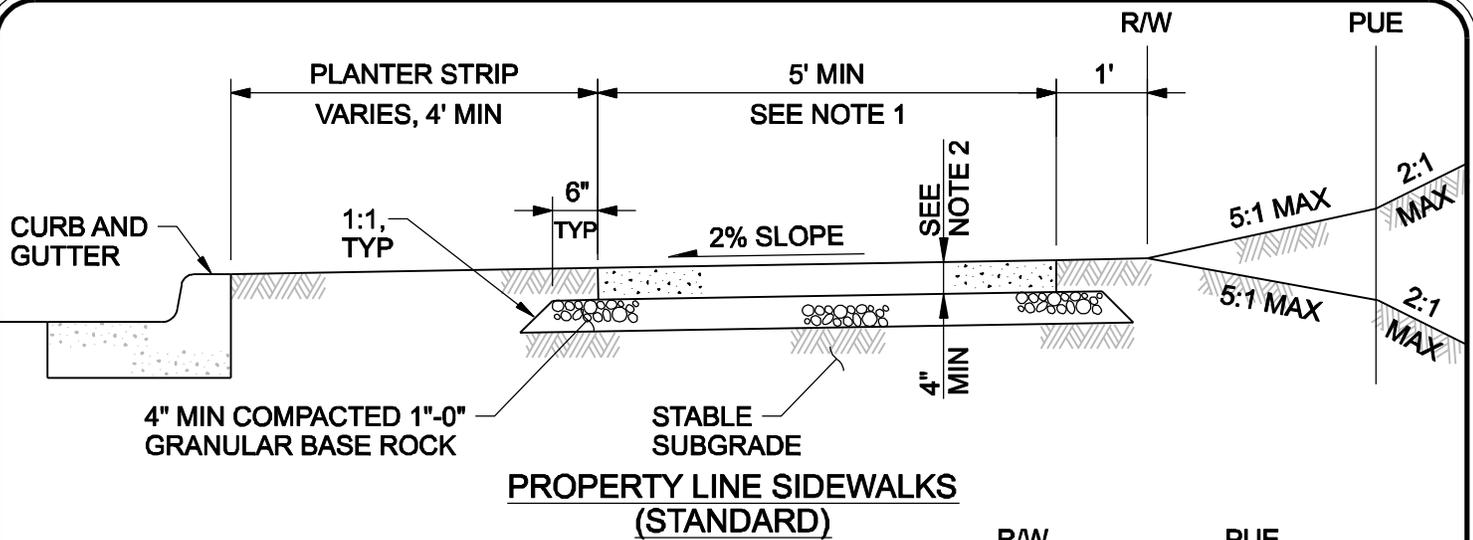
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STANDARD DRAWING TITLE

CURB DRAIN UNDER EXISTING SIDEWALKS

NO SCALE
 DIVISION
 STREET
 DRAWING NO.

338



STREET TYPE	PROPERTY LINE SIDEWALK WIDTH (STANDARD)	CURLINE SIDEWALK WIDTH (SEE NOTE 1)
LOCAL	5'-0"	6'-0" (SEE NOTE 1)
ARTERIALS AND COLLECTORS	6'-0" TO 8'-0", AS REQUIRED	7'-0" (SEE NOTE 1)

NOTES:

- CURLINE SIDEWALKS ARE NOT PERMITTED WITHOUT CITY ENGINEER APPROVAL. WHERE CURLINE SIDEWALKS ABUT HANDICAP PARKING STALLS, INCREASE WIDTHS SHOWN BY 1'-0".
- CONCRETE THICKNESS FOR SIDEWALKS SHALL BE NOMINAL 4" MIN. THICKNESS IN DRIVEWAYS SHALL BE 6" MIN. COMMERCIAL DRIVEWAYS SHALL BE 8" MIN.
- CONCRETE SHALL BE 3,300 PSI AT 28 DAY STRENGTH WITH 2" TO 4" SLUMP.
- PANELS TO BE 5' LONG, REGARDLESS OF PANEL WIDTH. CONTRACTION JOINTS SHALL BE PLACED AT ALL CHANGES IN DIRECTION, POINTS OF CURVATURE, AND AT 15' MAX INTERVALS.
- CURB DRAINS SHALL BE EXTENDED TO BACK OF SIDEWALK WITH 3" DIAMETER SCHEDULE 40 PVC PIPE AT 2% MIN SLOPE. CONTRACTION JOINT SHALL BE PLACED OVER PIPE.
- SIDEWALKS SHALL BE LOCATED ENTIRELY WITHIN RIGHT-OF-WAY OR SIDEWALK EASEMENTS, INCLUDING WIDENED SIDEWALKS AT DRIVEWAY APRONS.



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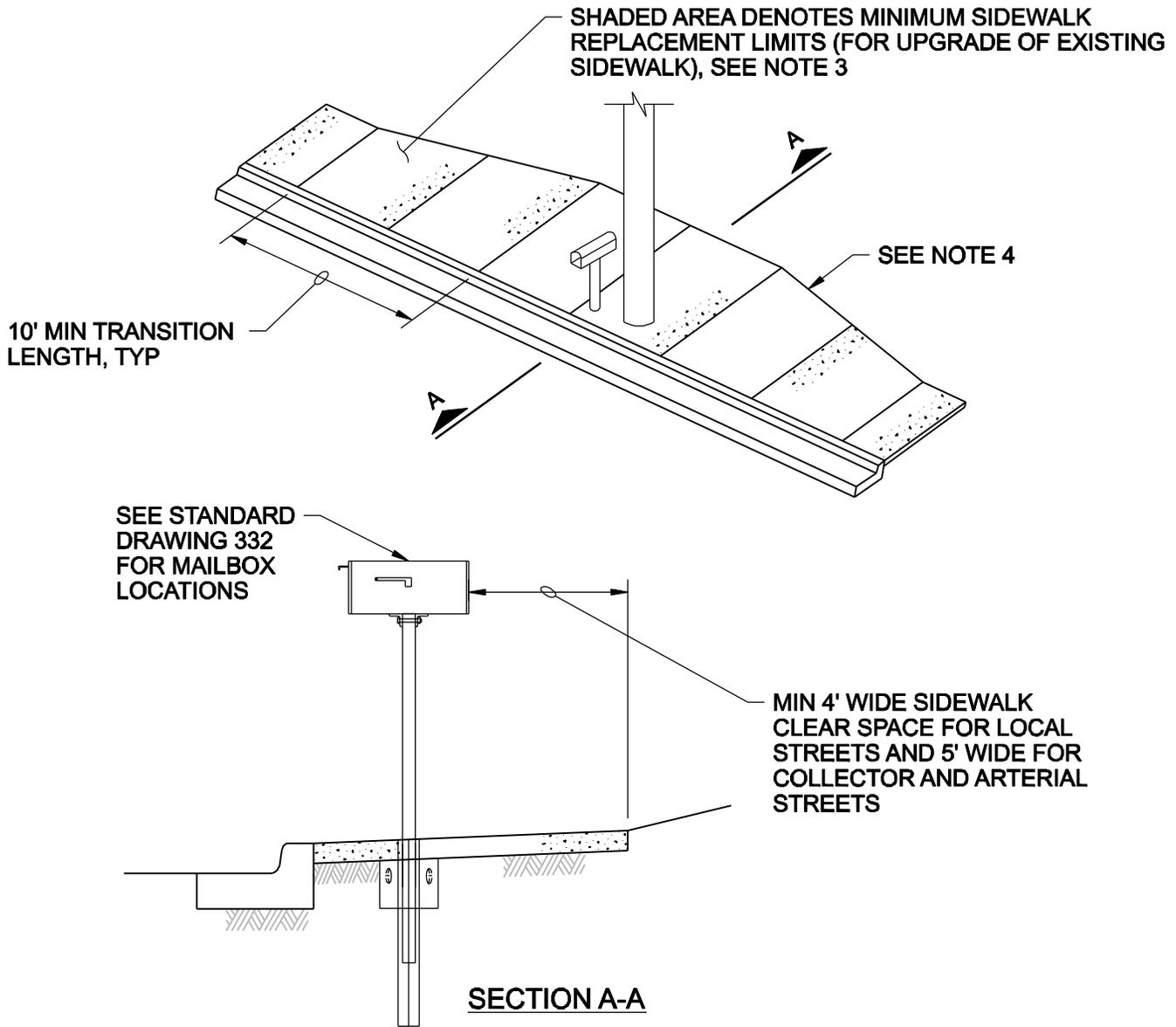
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STANDARD DRAWING TITLE

TYPICAL CONCRETE SIDEWALK

NO SCALE
 DIVISION
 STREET
 DRAWING NO.

340



NOTES:

1. PROVIDE MINIMUM SIDEWALK CLEAR SPACE AROUND EXISTING OBSTRUCTIONS IN NEW AND EXISTING NON-STANDARD CURBLINE SIDEWALK. COMMON OBSTRUCTIONS INCLUDE FIRE HYDRANTS, MAIL BOXES, UTILITY POLES AND OTHER OBJECTS THE MAY IMPEDE PEDESTRIAN TRAFFIC.
2. SEE STANDARD DRAWING 340 FOR MINIMUM SIDEWALK WIDTHS. SIDEWALKS AROUND OBSTRUCTIONS SHALL COMPLY WITH THE MOST CURRENT ADA GUIDELINES.
3. CONSTRUCTION OF ADDITIONAL SIDEWALK AGAINST THE BACK OF EXISTING SIDEWALK TO MEET MINIMUM CLEAR SPACE REQUIREMENTS IS NOT PERMITTED.
4. ADDITIONAL PUBLIC R/W OR SIDEWALK EASEMENT MAY BE REQUIRED TO MEET MINIMUM SIDEWALK WIDTH REQUIREMENTS.
5. THE PERMIT ISSUED FOR A SPECIFIC SITE WILL DETERMINE CONFIGURATION OF SIDEWALK INVOLVING NON-STANDARD DIMENSIONS AND RIGHT-OF-WAY LIMITATIONS.



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**TYPICAL
SIDEWALK AROUND
OBSTRUCTIONS**

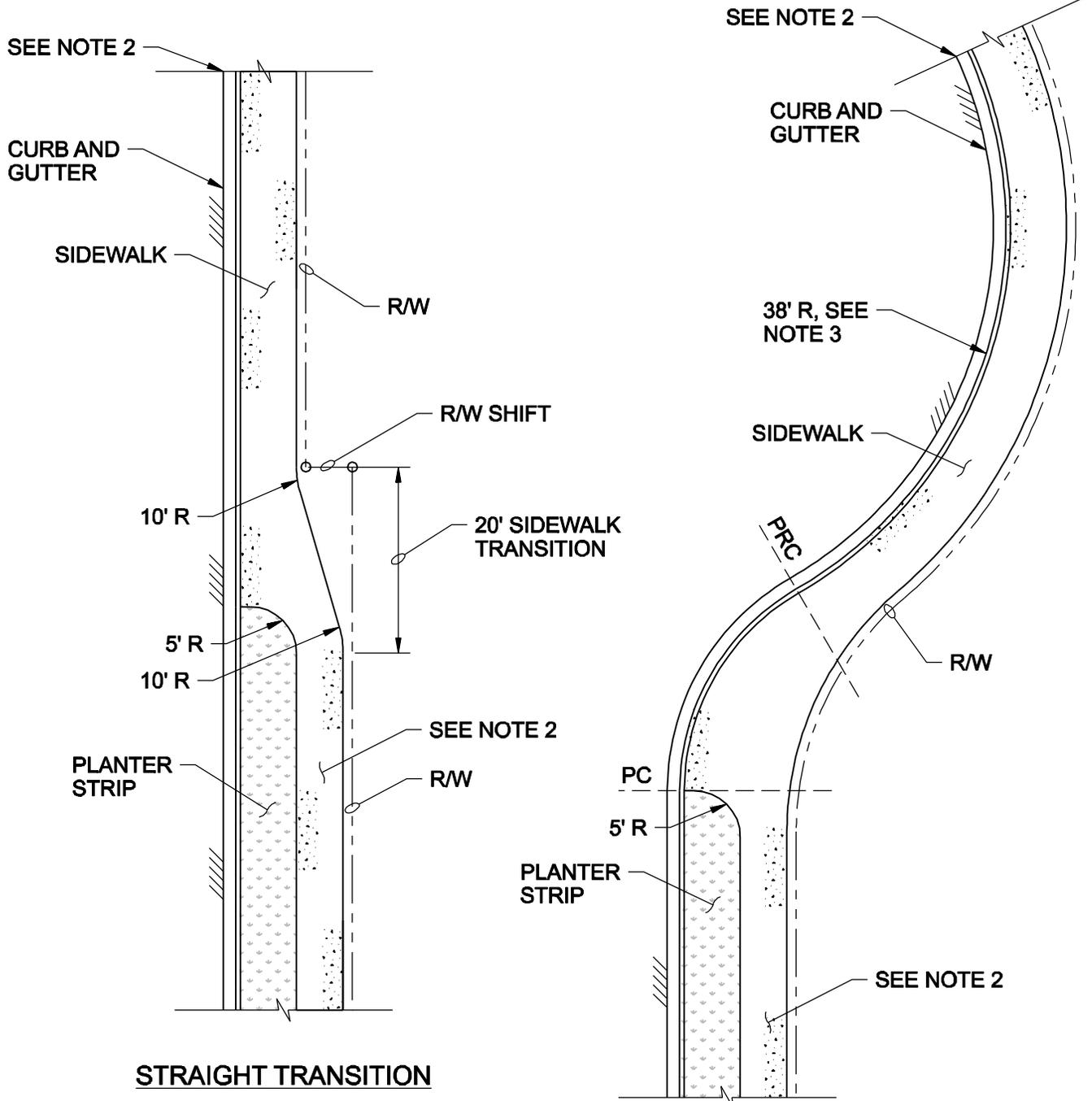
NO SCALE

DIVISION

STREET

DRAWING NO.

342



STRAIGHT TRANSITION

CUL-DE-SACS AND CURVE TRANSITION

NOTES:

1. CURBLINE SIDEWALK REQUIRES APPROVAL BY CITY ENGINEER. SEE PLANS FOR SPECIFIC PROJECTS WITH NON-TYPICAL TRANSITIONS FROM A STANDARD SIDEWALK WITH PLANTER STRIP TO CURBLINE SIDEWALK.
2. FOR TYPICAL SIDEWALKS, SEE STANDARD DRAWING 340.
3. CURB RADIUS SHOWN IS FOR TYPICAL CUL-DE-SAC. OTHER CURVES AS APPROVED BY CITY ENGINEER.



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**TYPICAL PROPERTY
LINE SIDEWALK TO
CURBLINE TRANSITION**

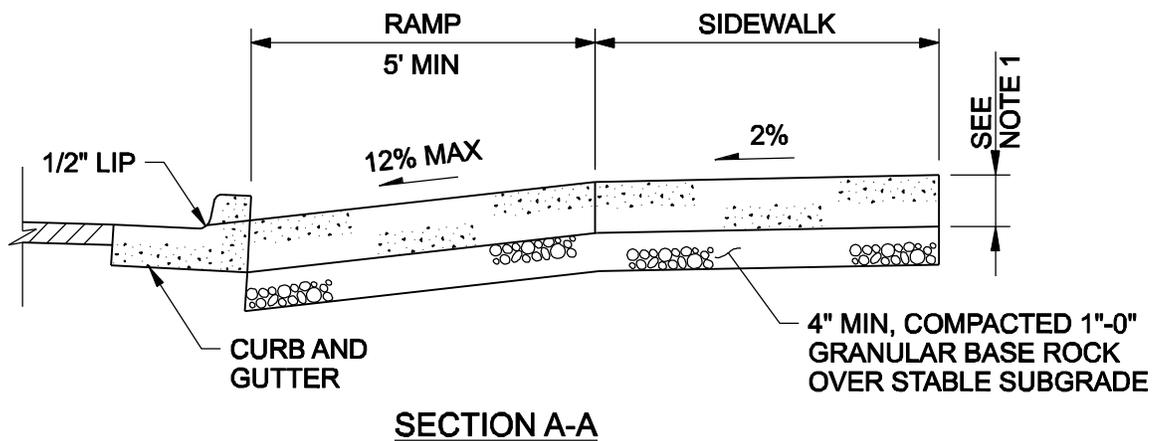
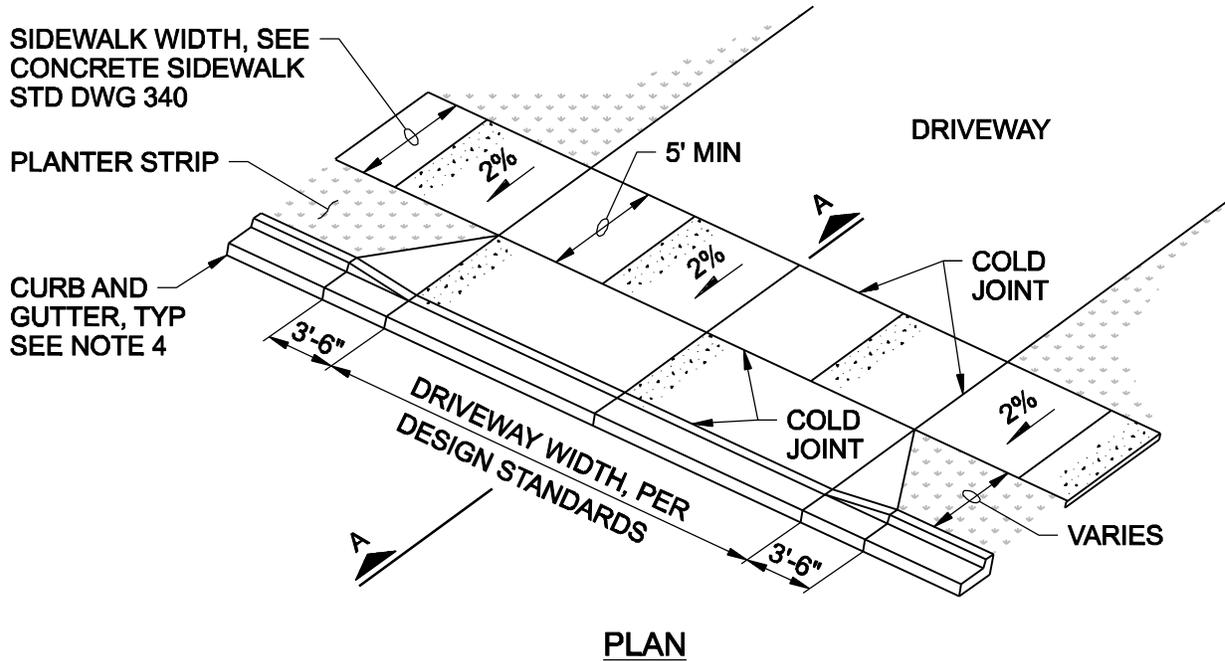
NO SCALE

DIVISION

STREET

DRAWING NO.

344



NOTES:

1. CONCRETE THICKNESS IN DRIVEWAYS SHALL BE 6" MIN. COMMERCIAL DRIVEWAYS SHALL BE 8" MIN. CONCRETE SHALL BE 3,300 PSI AT 28 DAY STRENGTH.
2. SIDEWALK PANELS TO BE 5' LONG. CONTRACTION JOINTS SHALL BE PLACED AT ALL CHANGES IN DIRECTION. DRIVEWAYS 15' FEET AND WIDER SHALL HAVE A CONTRACTION JOINT AT THE MIDPOINT.
3. WHEN THE EXISTING DRIVEWAY CANNOT BE MATCHED TO FIT THE NEW DRIVEWAY APPROACH WITHIN THE SLOPE LIMITATIONS SHOWN, ADJUST THE EXISTING DRIVEWAY (BEHIND THE SIDEWALK), NOT THE CURB, RAMP, OR SIDEWALK GRADE. COORDINATE WITH CITY ENGINEER.
4. CHECK GUTTER FLOW DEPTH AT DRIVEWAY LOCATION TO ASSURE THAT THE DESIGN FLOOD DOES NOT OVERTOP THE BACK OF THE SIDEWALK AT THE DRIVEWAY. IF OVERTOPPING WILL OCCUR, PLACE AN INLET AT THE UPSTREAM SIDE OF THE DRIVEWAY OR PERFORM OTHER APPROVED DESIGN MITIGATION.

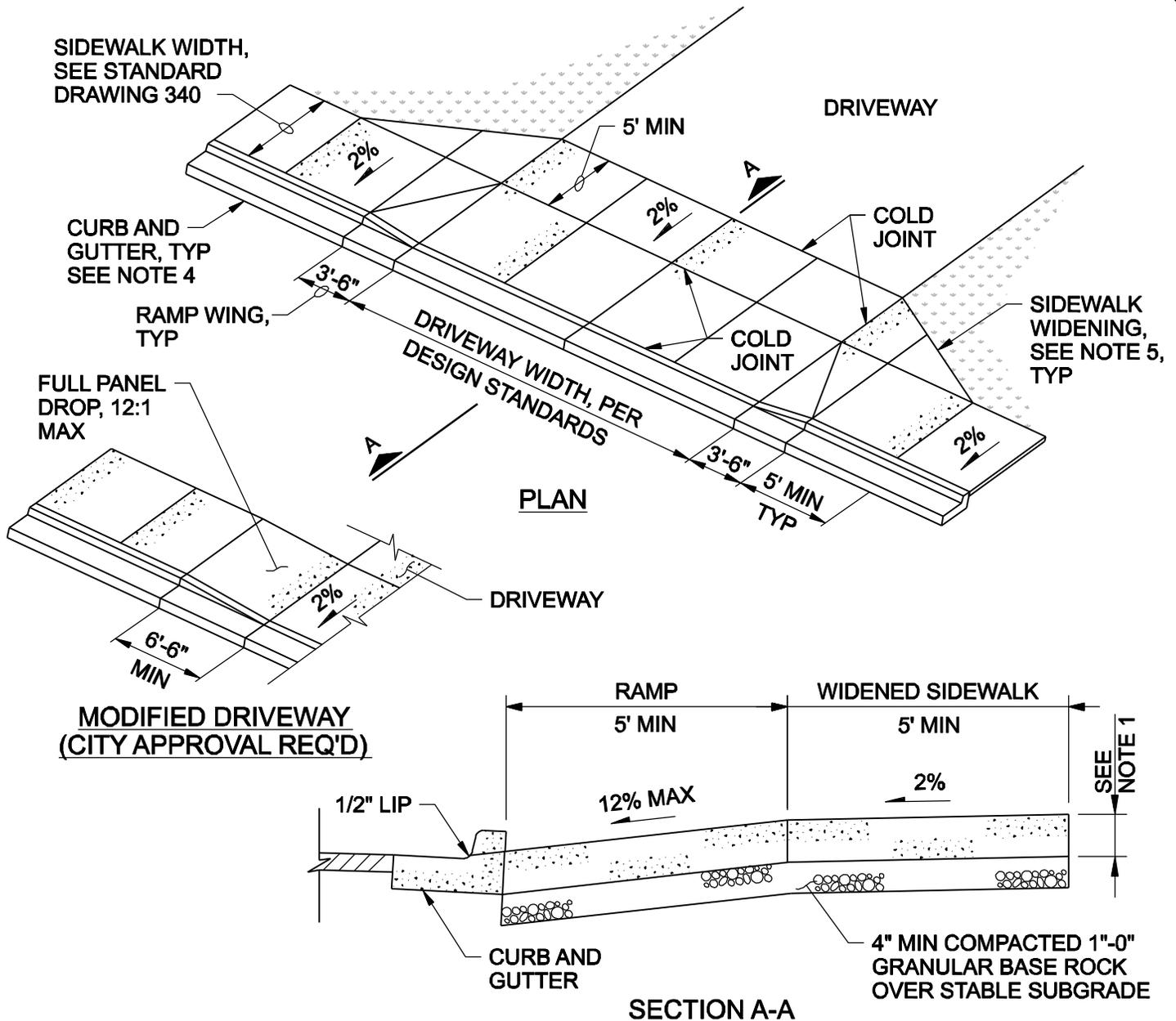


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STANDARD DRAWING TITLE
**DRIVEWAY WITH
PROPERTY LINE
SIDEWALKS**

NO SCALE
DIVISION
STREET
DRAWING NO.
346



NOTES:

1. CONCRETE THICKNESS IN DRIVEWAYS SHALL BE 6" MIN. COMMERCIAL DRIVEWAYS SHALL BE 8" MIN. CONCRETE SHALL BE 3,300 PSI AT 28 DAY STRENGTH.
2. SIDEWALK PANELS TO BE 5' LONG. CONTRACTION JOINTS SHALL BE PLACED AT ALL CHANGES IN DIRECTION. DRIVEWAYS 15' FEET AND WIDER SHALL HAVE A CONTRACTION JOINT AT THE MIDPOINT.
3. WHEN THE EXISTING DRIVEWAY CANNOT BE MATCHED TO FIT THE NEW DRIVEWAY APPROACH WITHIN THE SLOPE LIMITATIONS SHOWN, ADJUST THE EXISTING DRIVEWAY (BEHIND THE SIDEWALK), NOT THE CURB, RAMP, OR SIDEWALK GRADE. COORDINATE WITH CITY ENGINEER.
4. CHECK GUTTER FLOW DEPTH AT DRIVEWAY LOCATION TO ASSURE THAT THE DESIGN FLOOD DOES NOT OVERTOP THE BACK OF THE SIDEWALK AT THE DRIVEWAY. IF OVERTOPPING WILL OCCUR, PLACE AN INLET AT THE UPSTREAM SIDE OF THE DRIVEWAY OR PERFORM OTHER APPROVED DESIGN MITIGATION.
5. ADDITIONAL PUBLIC RIGHT OF WAY OR SIDEWALK EASEMENT MAY BE REQUIRED TO MEET MINIMUM SIDEWALK WIDTH REQUIREMENTS.

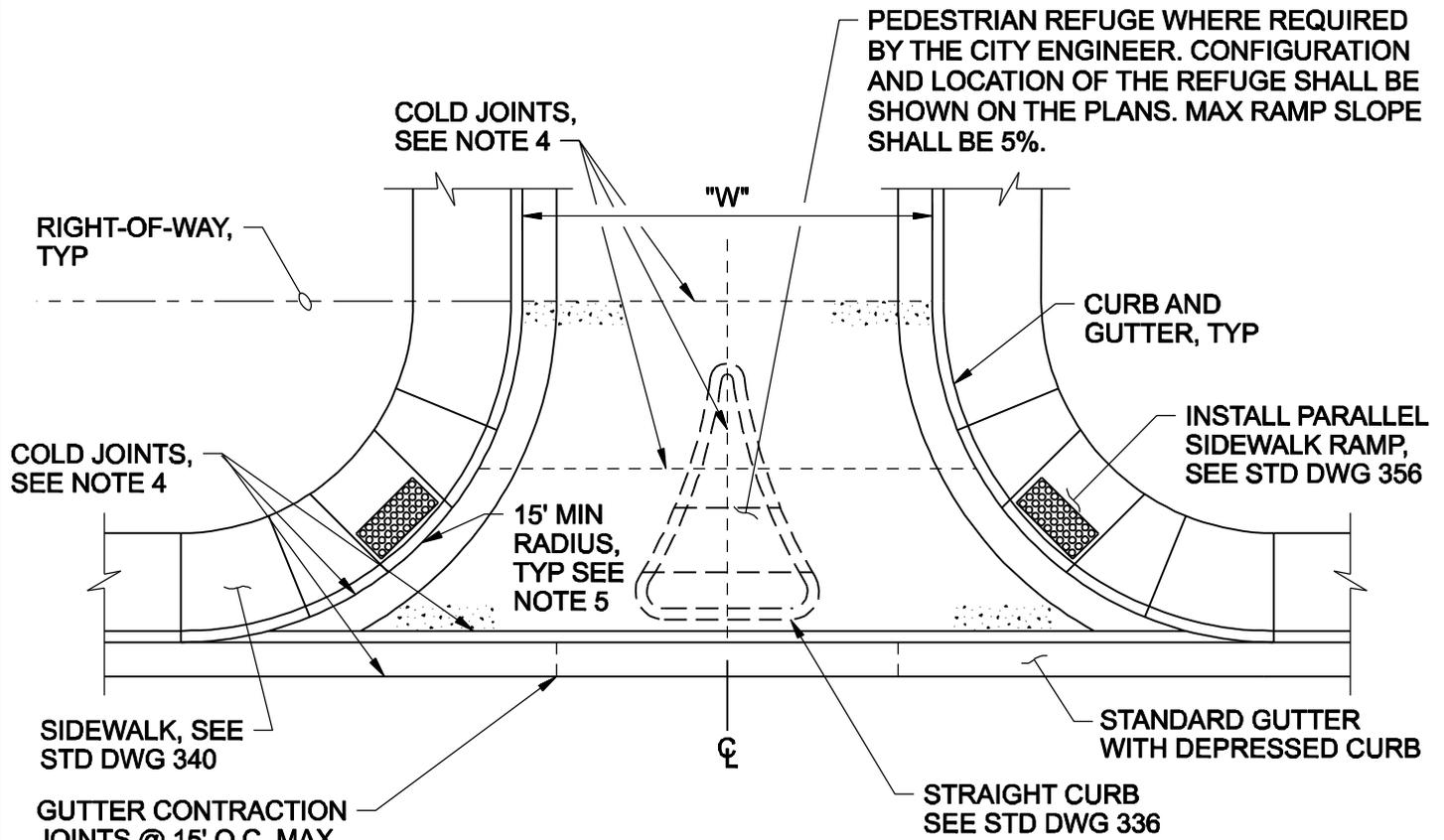


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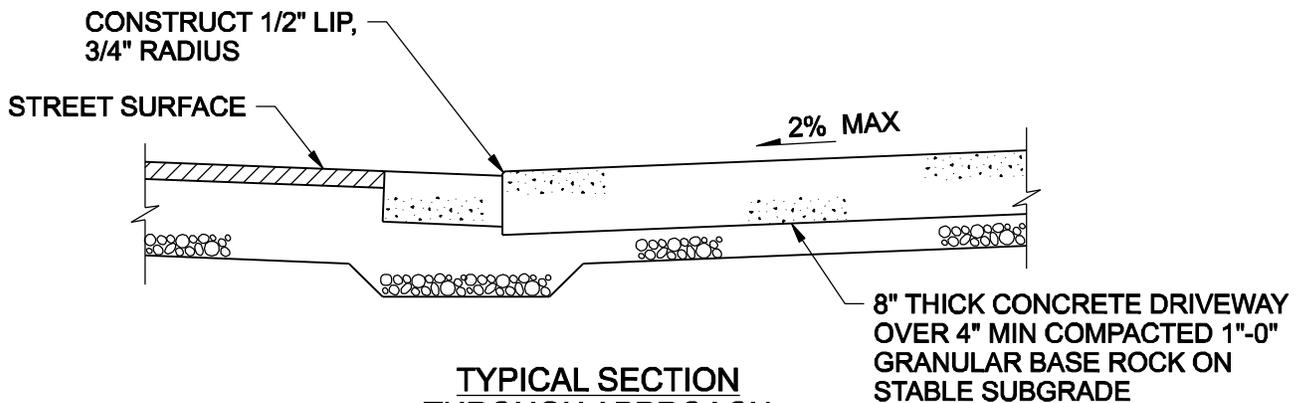
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STANDARD DRAWING TITLE
DRIVEWAY WITH CURBLINE SIDEWALKS

NO SCALE
DIVISION
STREET
DRAWING NO.
348



PLAN



TYPICAL SECTION THROUGH APPROACH

NOTES:

1. THIS TYPE OF APPROACH SHALL NOT BE USED WITHOUT APPROVAL OF THE CITY ENGINEER.
2. "W" = WIDTH REQUIREMENTS AS SPECIFIED IN THE DEVELOPMENT CODE, OR AS INDICATED ON PLANS.
3. ALL CONCRETE FOR COMMERCIAL DRIVEWAY TO HAVE A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 5,000 PSI.
4. PROVIDE COLD JOINTS AS SPECIFIED AT 15' O.C. MAX.
5. LARGER RADIUS MAY BE REQUIRED FOR BUS AND TRUCK TRAFFIC, AS APPROVED BY CITY ENGINEER.



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**CURB RETURN
FOR COMMERCIAL
DRIVEWAY APPROACH**

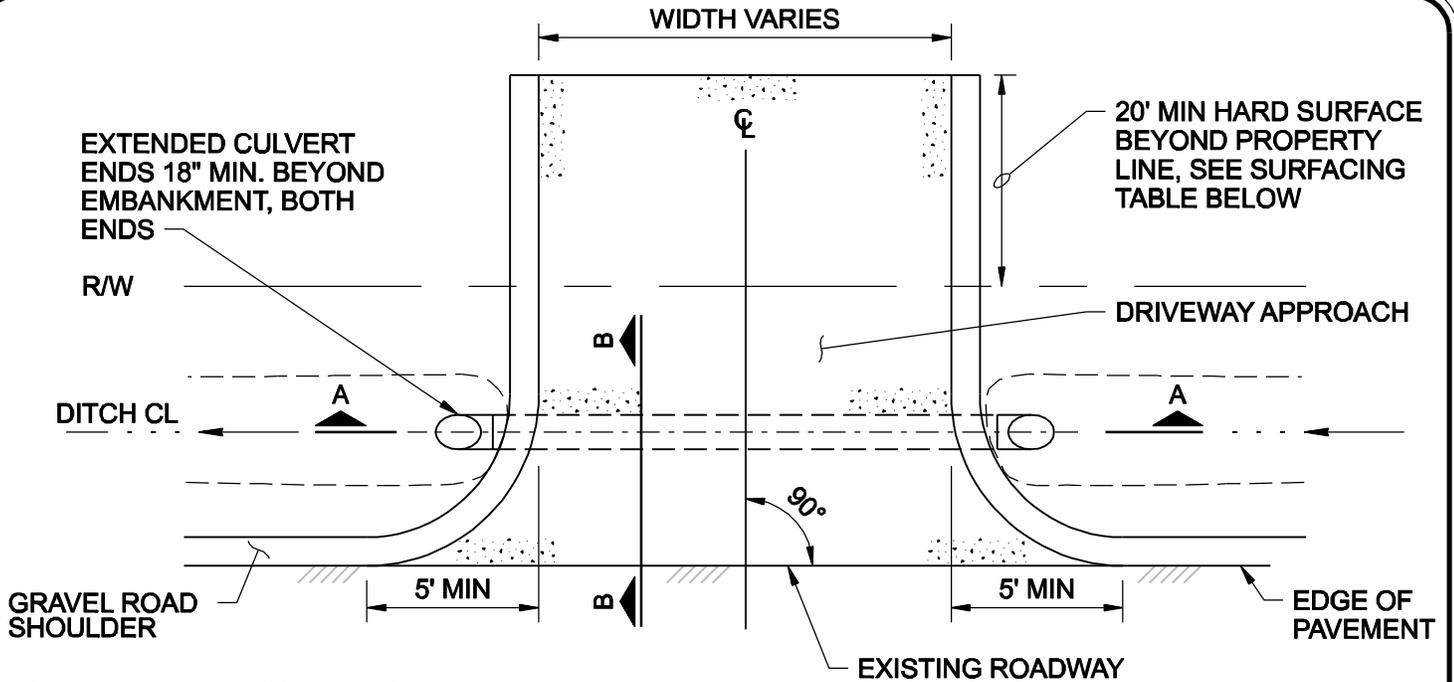
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DIVISION

STREET

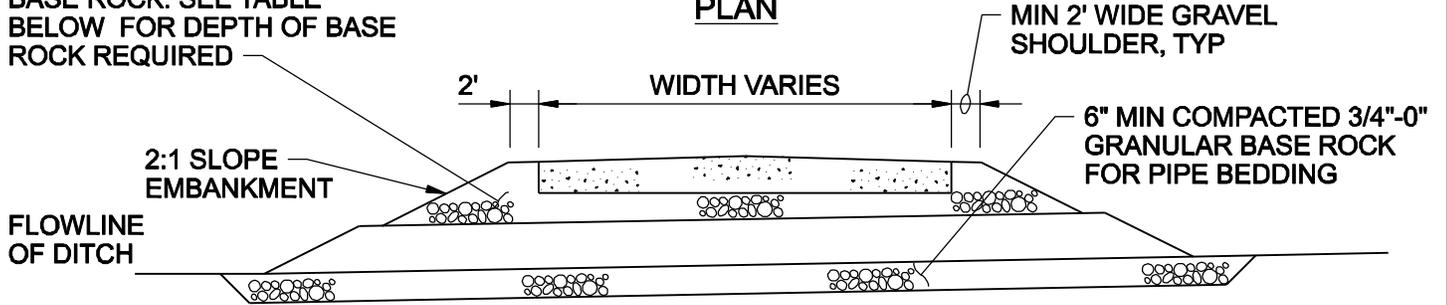
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350



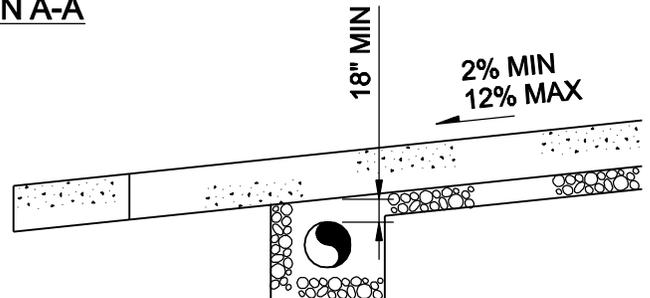
COMPACTED 1"-0" GRANULAR BASE ROCK. SEE TABLE BELOW FOR DEPTH OF BASE ROCK REQUIRED

PLAN



SECTION A-A

SURFACING MATERIAL	SURFACING REQUIREMENTS	BASE ROCK REQUIREMENTS
CONCRETE	6" MIN 3,300 PSI PCC PVMT	4" MIN OF 1"-0" GRANULAR BASE ROCK
ASPHALT CONCRETE PAVEMENT	3" MIN ASPHALT CONC PVMT	8" MIN OF 1"-0" GRANULAR BASE ROCK



SECTION B-B

NOTES:

1. ALL SURFACE MATERIALS, BASE ROCK, AND TRENCH BACKFILL SHALL BE COMPACTED AS SPECIFIED.
2. CONCRETE SHALL HAVE A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 3,300 PSI.
3. ROADSIDE DITCHES SHALL NOT BE FILLED IN EXCEPT AT DRIVEWAY LOCATIONS WITH CULVERT.



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**TYPICAL DRIVEWAY
APPROACH FOR
UNIMPROVED STREETS**

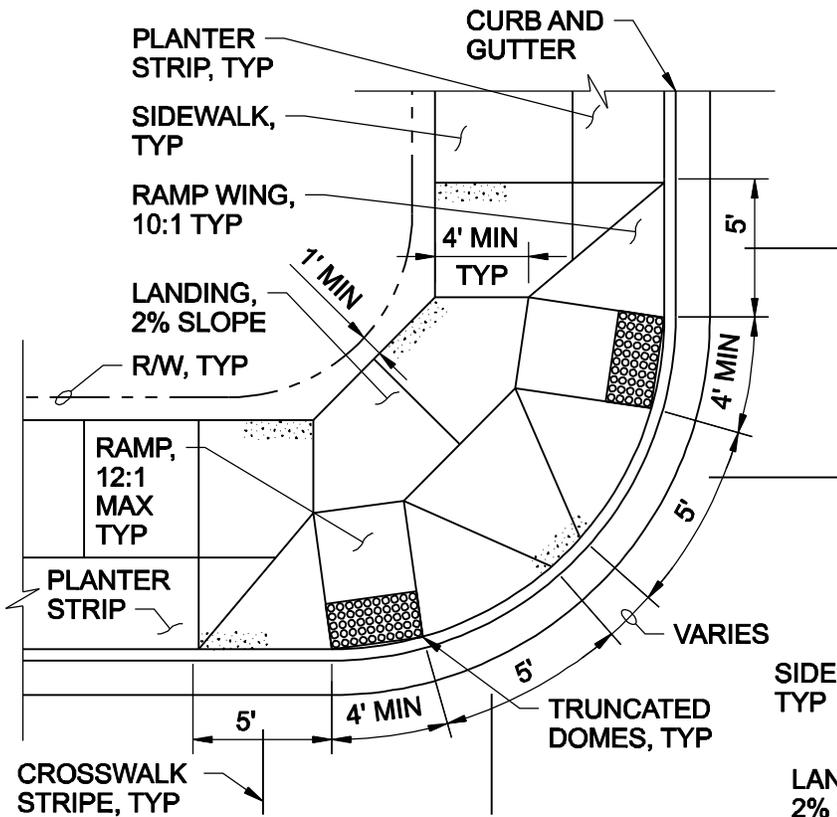
NO SCALE

DIVISION

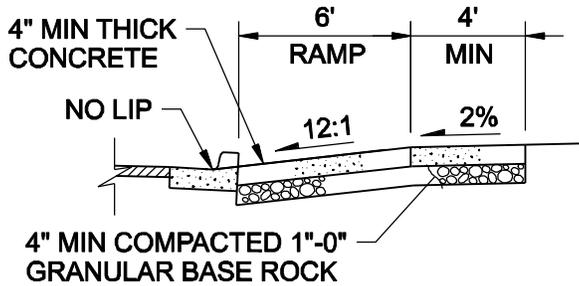
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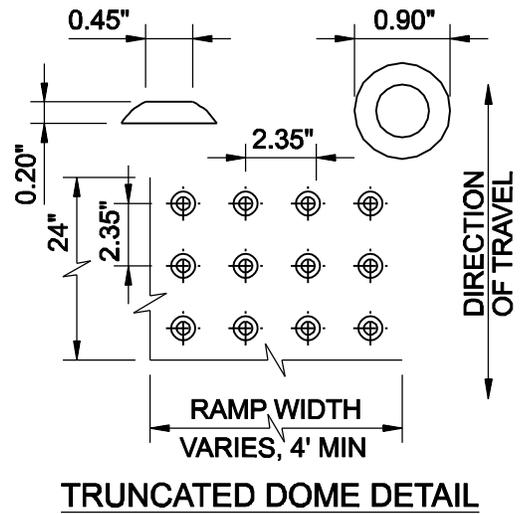
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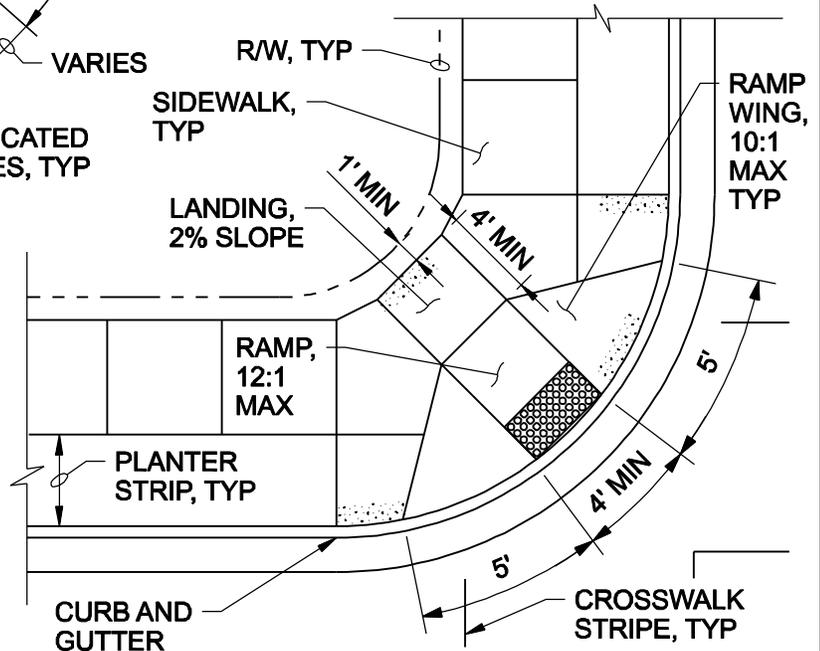
INTERSECTION RAMP FOR ARTERIAL AND COLLECTOR STREETS



TYPICAL SECTION THROUGH RAMP



TRUNCATED DOME DETAIL



INTERSECTION RAMP FOR LOCAL RESIDENTIAL STREETS

NOTES:

1. RAMPS AND SIDEWALKS SHOWN SHADED ARE TO BE CONSTRUCTED WITH THE STREET IMPROVEMENTS.
2. CONCRETE SHALL BE 3,300 PSI AT 28-DAY STRENGTH.
3. DETECTABLE WARNING PANEL SHALL BE CAST-IN-TACT CONCRETE TRUNCATED DOME PANEL BY MASONS SUPPLY CO. PANELS TO BE RED IN COLOR. INSTALL PER MANUFACTURER'S RECOMMENDATIONS. MAINTAIN A MINIMUM 4" THICK CONCRETE UNDER THE DETECTABLE WARNING PANEL.
4. ALL RAMPS AND TRANSITIONS SHALL BE ADA COMPLIANT. A 4' MIN LANDING SHALL BE PLACED AT THE TOP OF EACH RAMP. LANDING SLOPES SHALL NOT EXCEED 2% IN ANY DIRECTION.
5. NO UTILITY STRUCTURES (CLEANOUTS, VALVE BOXES, ETC) ARE PERMITTED WITHIN RAMP AREA.

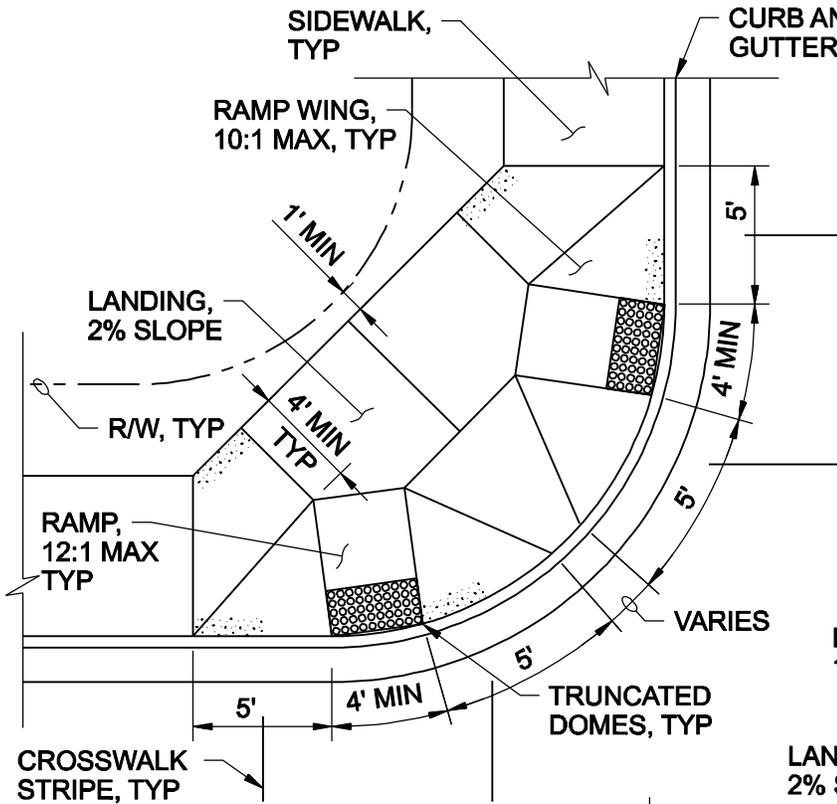


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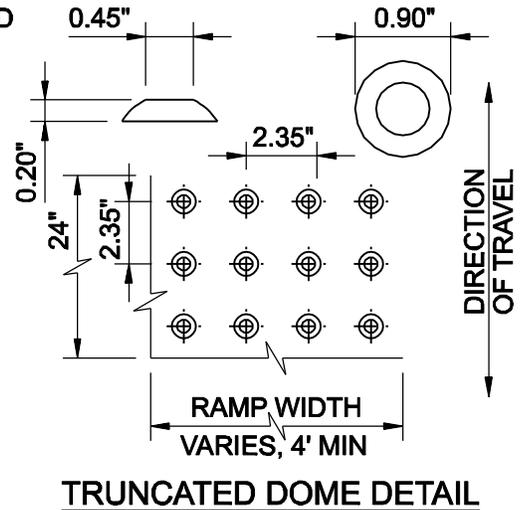
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STANDARD DRAWING TITLE
INTERSECTION RAMPS FOR PROPERTY LINE SIDEWALKS

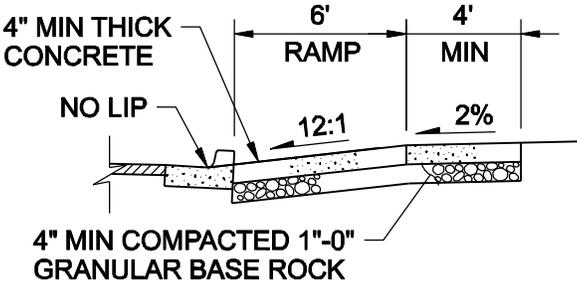
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STREET
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354



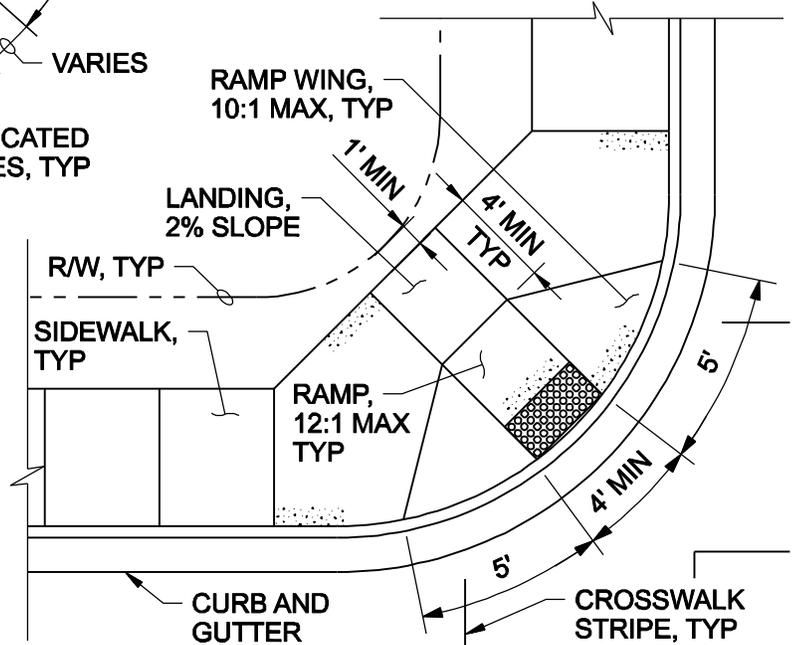
INTERSECTION RAMP FOR ARTERIAL AND COLLECTOR STREETS



TRUNCATED DOME DETAIL



TYPICAL SECTION THROUGH RAMP



INTERSECTION RAMP FOR LOCAL RESIDENTIAL STREETS

NOTES:

1. RAMPS AND SIDEWALKS SHOWN SHADED ARE TO BE CONSTRUCTED WITH THE STREET IMPROVEMENTS.
2. CONCRETE SHALL BE 3,300 PSI AT 28-DAY STRENGTH.
3. DETECTABLE WARNING PANEL SHALL BE CAST IN PLACE CONCRETE TRUNCATED DOME PANEL BY MASONS SUPPLY CO. PANELS TO BE RED IN COLOR. INSTALL PER MANUFACTURER'S RECOMMENDATIONS. MAINTAIN A MINIMUM 4" THICK CONCRETE UNDER THE DETECTABLE WARNING PANEL.
4. ALL RAMPS AND TRANSITIONS SHALL BE ADA COMPLIANT. A 4' MIN LANDING SHALL BE PLACED AT THE TOP OF EACH RAMP. LANDING SLOPES SHALL NOT EXCEED 2% IN ANY DIRECTION.
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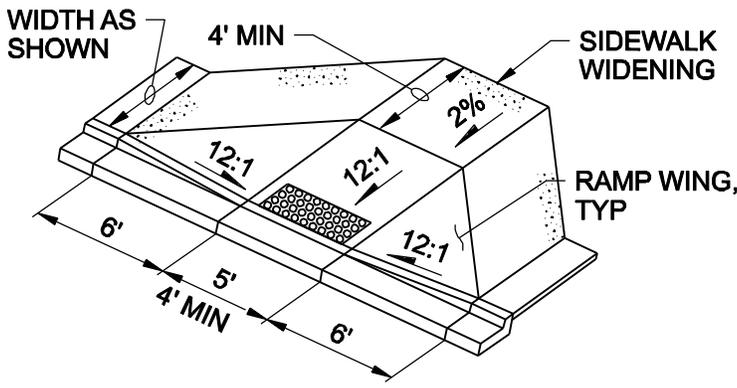


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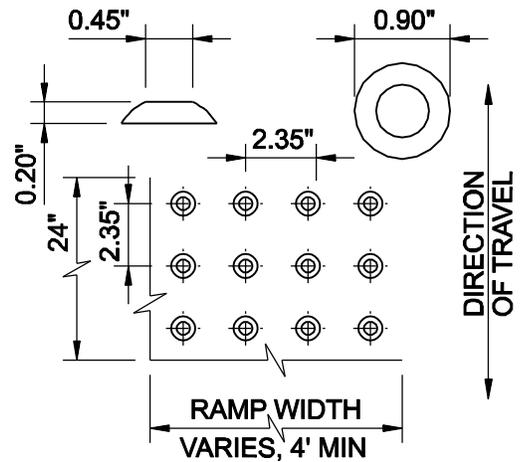
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STANDARD DRAWING TITLE
INTERSECTION RAMPS FOR CURBLINE SIDEWALKS

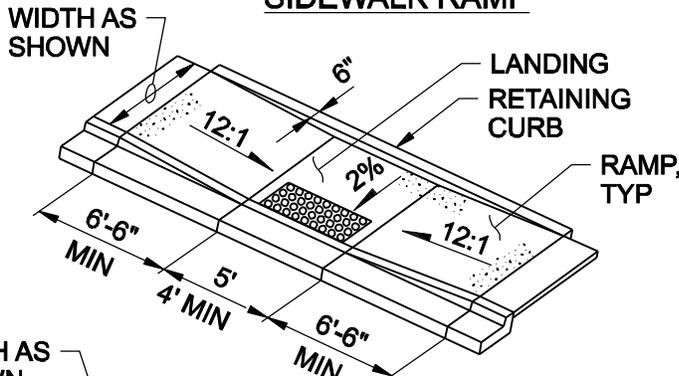
NO SCALE
DIVISION
STREET
DRAWING NO.
356



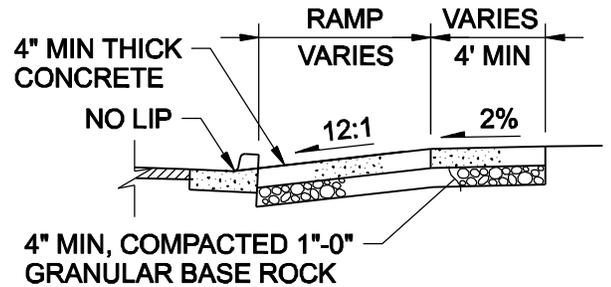
**CURLINE
SIDEWALK RAMP**



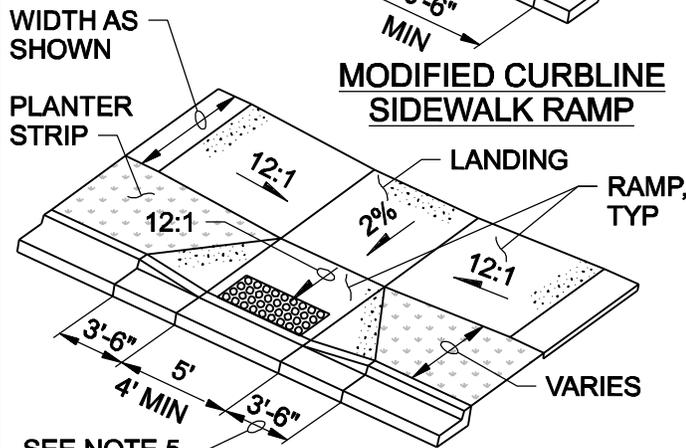
TRUNCATED DOME DETAIL



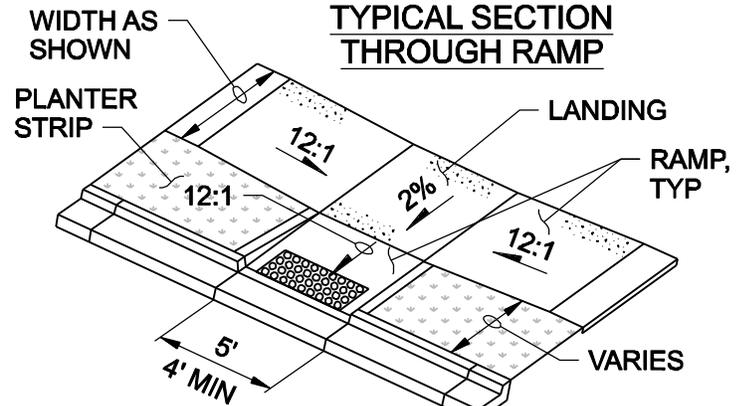
**MODIFIED CURLINE
SIDEWALK RAMP**



**TYPICAL SECTION
THROUGH RAMP**



**STANDARD PROPERTY LINE
SIDEWALK RAMP**



**MODIFIED PROPERTY LINE
SIDEWALK RAMP**

NOTES:

1. RAMPS AND SIDEWALKS SHOWN SHADED ARE TO BE CONSTRUCTED WITH THE STREET IMPROVEMENTS. CONCRETE SHALL BE 3,300 PSI AT 28-DAY STRENGTH. SEE STANDARD DRAWING 340 FOR SIDEWALK WIDTHS AND ADDITIONAL NOTES.
2. DETECTABLE WARNING PANEL SHALL BE CAST-IN-TACT CONCRETE TRUNCATED DOME PANEL BY MASONS SUPPLY CO. PANELS TO BE RED IN COLOR. INSTALL PER MANUFACTURER'S RECOMMENDATIONS. MAINTAIN A MINIMUM 4" THICK CONCRETE UNDER THE DETECTABLE WARNING PANEL.
3. ALL RAMPS AND TRANSITIONS SHALL BE ADA COMPLIANT. A 4' MIN LANDING SHALL BE PLACED AT THE TOP OF EACH RAMP WHERE SHOWN. LANDING SLOPES SHALL NOT EXCEED 2% IN ANY DIRECTION.
4. NO UTILITY STRUCTURES (CLEANOUTS, VALVE BOXES, ETC) ARE PERMITTED WITHIN RAMP AREA.
5. IF PLANTER STRIP IS CONCRETE OR ASPHALT CONCRETE PAVEMENT, RAMP WING SHALL BE 12:1 MAX.



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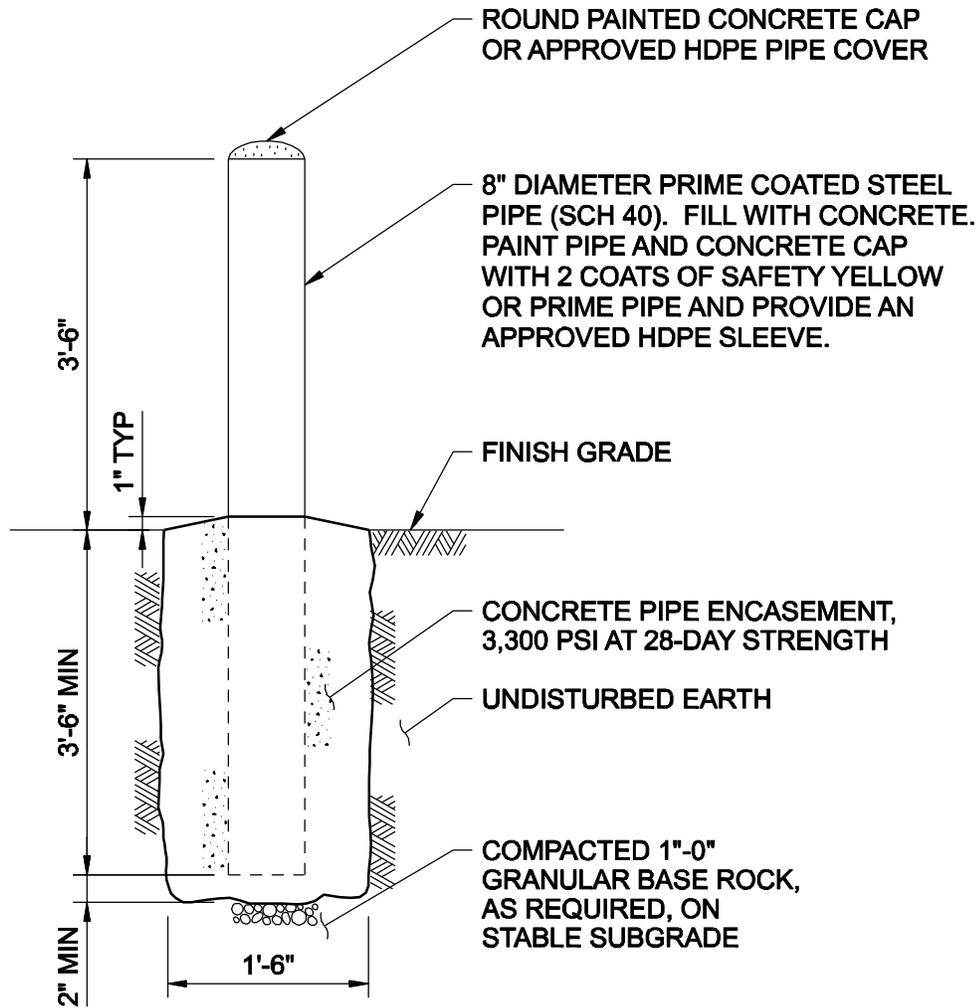
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STANDARD DRAWING TITLE

**PARALLEL
SIDEWALK RAMPS**

NO SCALE
DIVISION
STREET
DRAWING NO.

358



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STANDARD DRAWING TITLE

TYPICAL
BOLLARD

NO SCALE

DIVISION

STREET

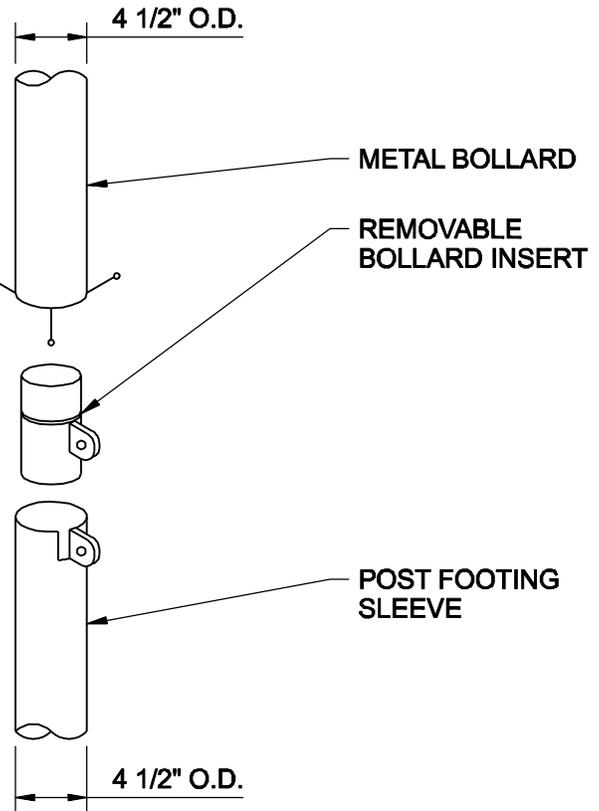
DRAWING NO.

360

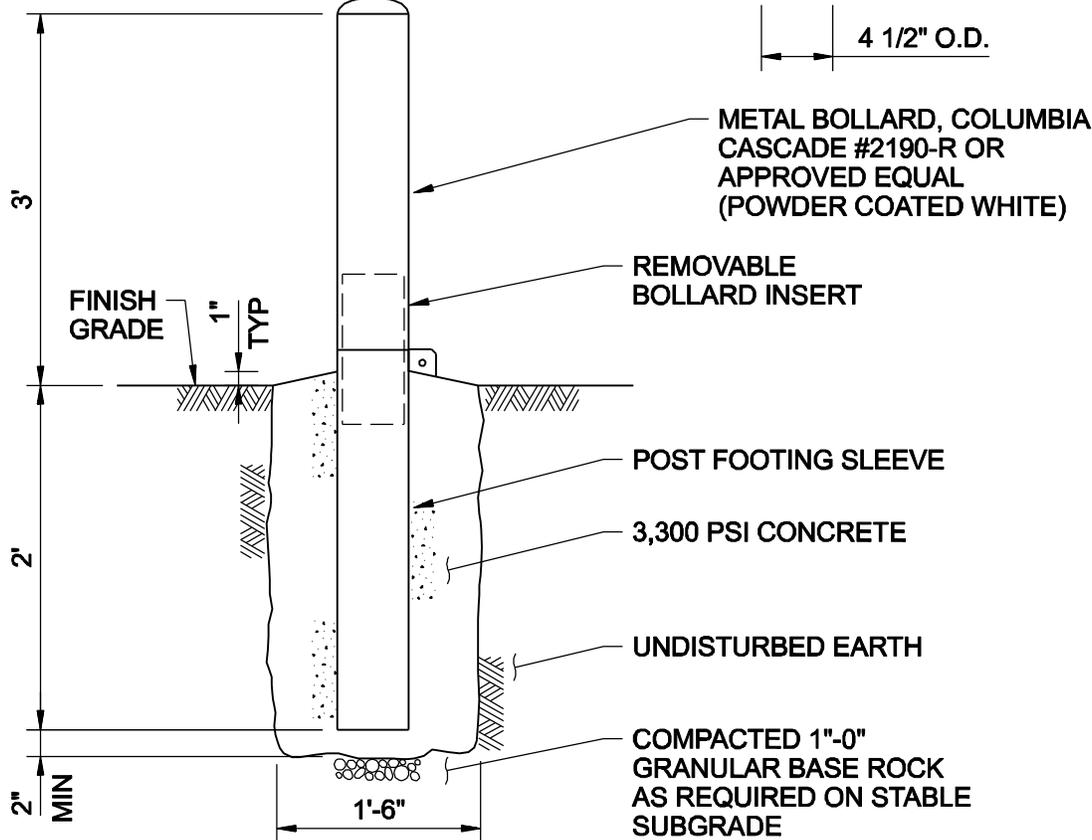
BOLLARD WITH REMOVABLE BOLLARD INSERT WITH 3 EA 3/8-16x3/8" LONG HEX SOCKET SET SCREWS

INSTALLATION SEQUENCE:

1. DIG FOOTING HOLE AND SET POST FOOTING SLEEVE PLUM AND SQUARE IN CONCRETE
2. INSERT REMOVABLE BOLLARD INSERT INTO BOLLARD.
3. TIGHTEN HEX SOCKET SET SCREWS.
4. SET THE BOLLARD AND REMOVABLE BOLLARD INSERT INTO THE POST FOOTING SLEEVE.



POST W/ DOME TOP



NOTES:

1. LOCK TAB TO BE ORIENTED OPPOSITE THE DIRECTION OF VEHICULAR ACCESS.
2. DECORATIVE STANDARD BOLLARD MAY BE APPROVED BY CITY ENGINEER.

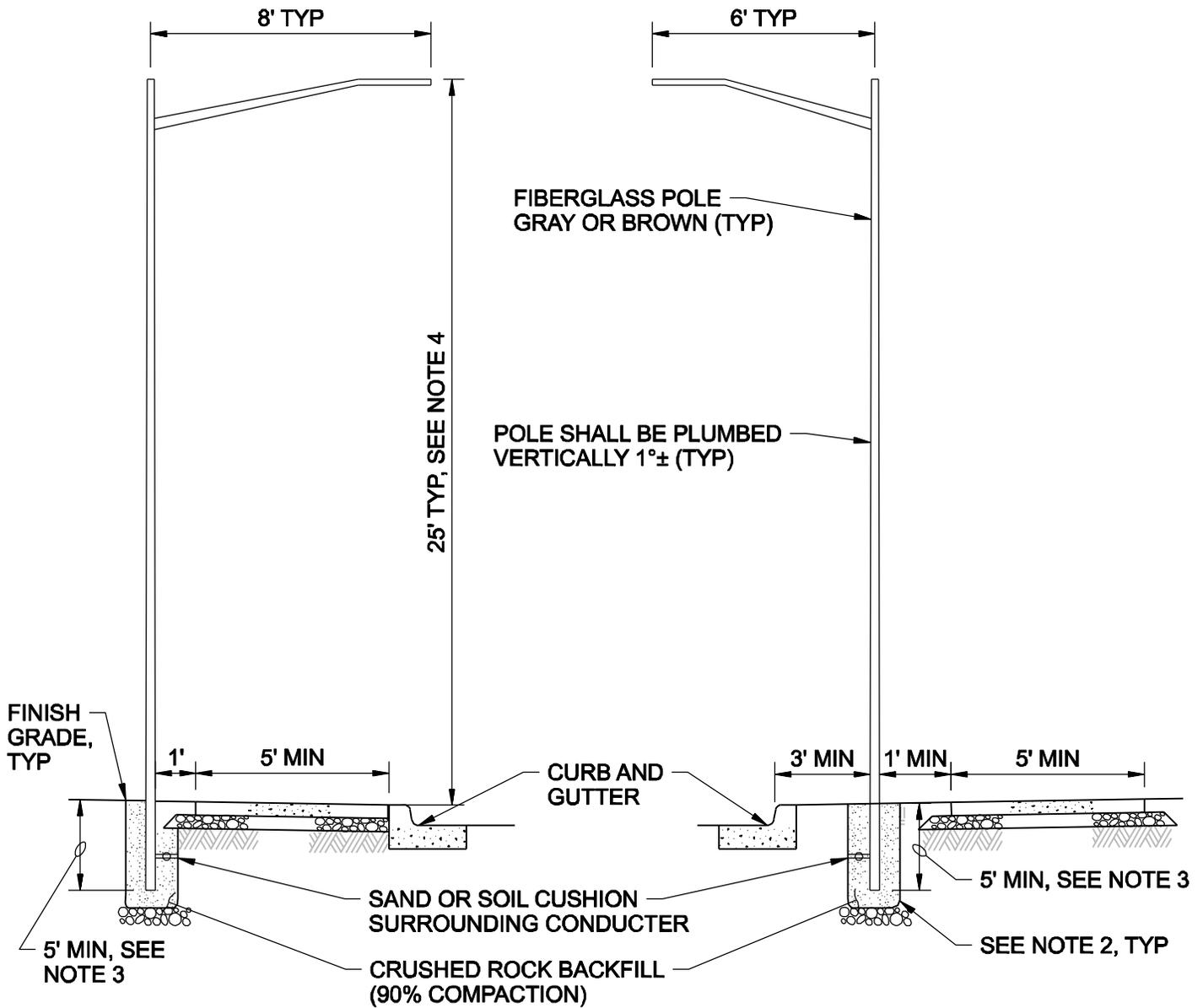


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STANDARD DRAWING TITLE
REMOVABLE BOLLARD

NO SCALE
DIVISION
STREET
DRAWING NO.
362



**TYPICAL STREET LIGHT POLE
CURBLINE SIDEWALK**

**TYPICAL STREET LIGHT POLE
PROPERTY LINE SIDEWALKS**

NOTES:

1. CONTRACTOR TO COORDINATE STREET LIGHT INSTALLATION WITH PACIFIC POWER COMPANY.
2. MINIMUM HOLE DIAMETER TO BE 2x THE POLE BASE DIAMETER.
3. MINIMUM DIRECT BURIAL DEPTH EQUAL TO 5', OR GREATER AS REQUIRED BY MANUFACTURER.
4. FOR MAJOR COLLECTOR AND ARTERIAL STREETS, POLE HEIGHT SHALL BE 30' FROM FINISH GRADE.
5. DECORATIVE STREET LIGHT LOCATION SHALL BE AS DETERMINED BY CITY ENGINEER.



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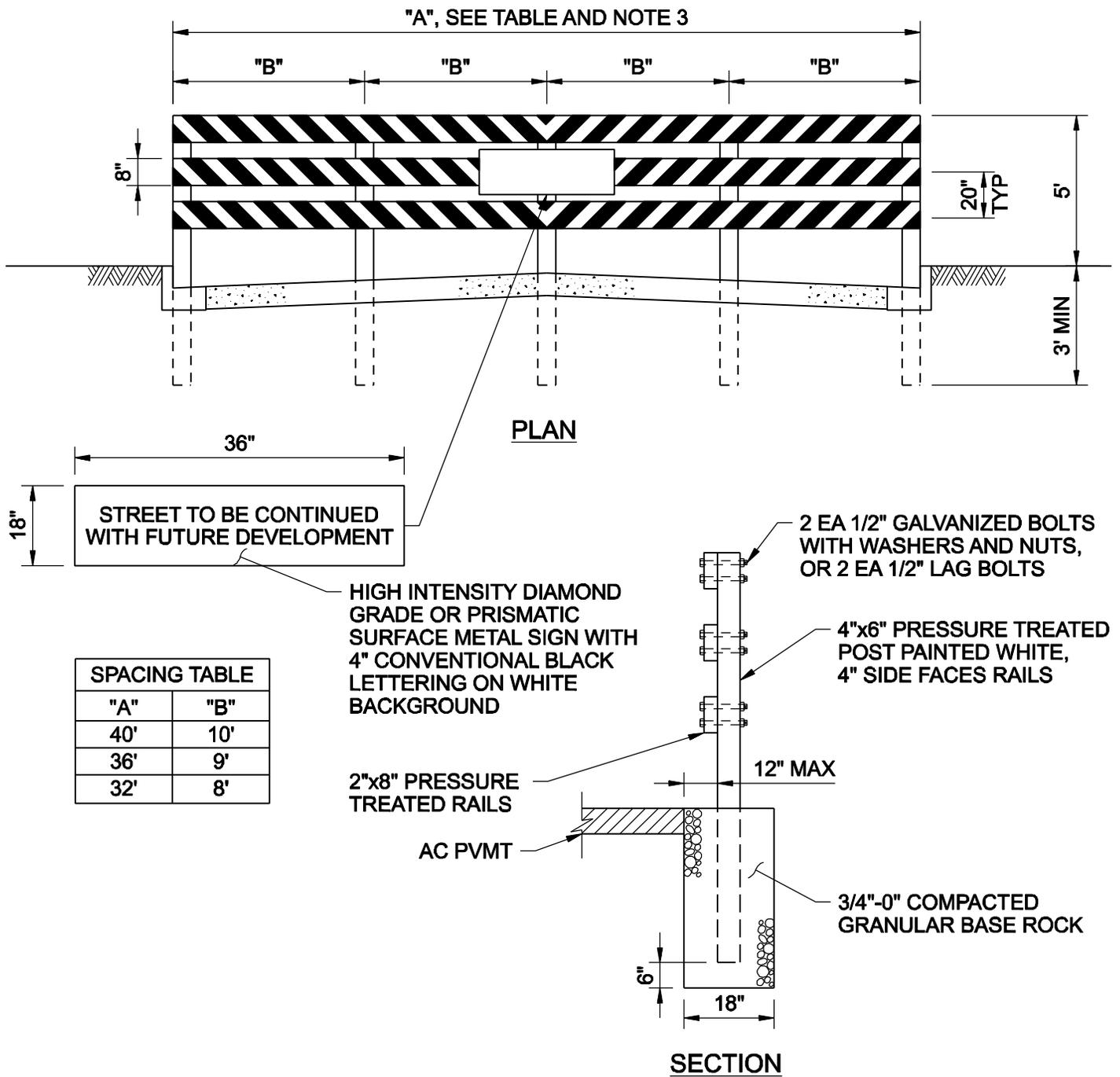
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STANDARD DRAWING TITLE

**TYPICAL STREET
LIGHT POLE**

NO SCALE
DIVISION
STREET
DRAWING NO.

364



SPACING TABLE	
"A"	"B"
40'	10'
36'	9'
32'	8'

HIGH INTENSITY DIAMOND GRADE OR PRISMATIC SURFACE METAL SIGN WITH 4" CONVENTIONAL BLACK LETTERING ON WHITE BACKGROUND

2"x8" PRESSURE TREATED RAILS

AC PVMT

2 EA 1/2" GALVANIZED BOLTS WITH WASHERS AND NUTS, OR 2 EA 1/2" LAG BOLTS

4"x6" PRESSURE TREATED POST PAINTED WHITE, 4" SIDE FACES RAILS

3/4"-0" COMPACTED GRANULAR BASE ROCK

NOTES:

1. STRIPING SHALL BE ALTERNATING RED (RODDA #1249 OR EQUAL) AND WHITE STRIPES 6" IN WIDTH AT A 45-DEGREE ANGLE AND SHALL BE EITHER RETRO-REFLECTIVE TAPE OR PAINTED WITH A SEALED RETRO-REFLECTIVE SURFACE.
2. ALL SIGNS SHALL CONFORM TO THE CURRENT VERSION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD). THIS BARRICADE SHALL CONFORM TO SECTION 3F-1 OF THE MUTCD.
3. FOR STREETS THAT DO NOT FIT STANDARD WIDTHS, EQUAL SPACING BETWEEN POSTS SHALL BE MAINTAINED NOT TO EXCEED 10' AND SHALL BE CENTERED ON STREET CENTERLINE.



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STREET BARRICADE

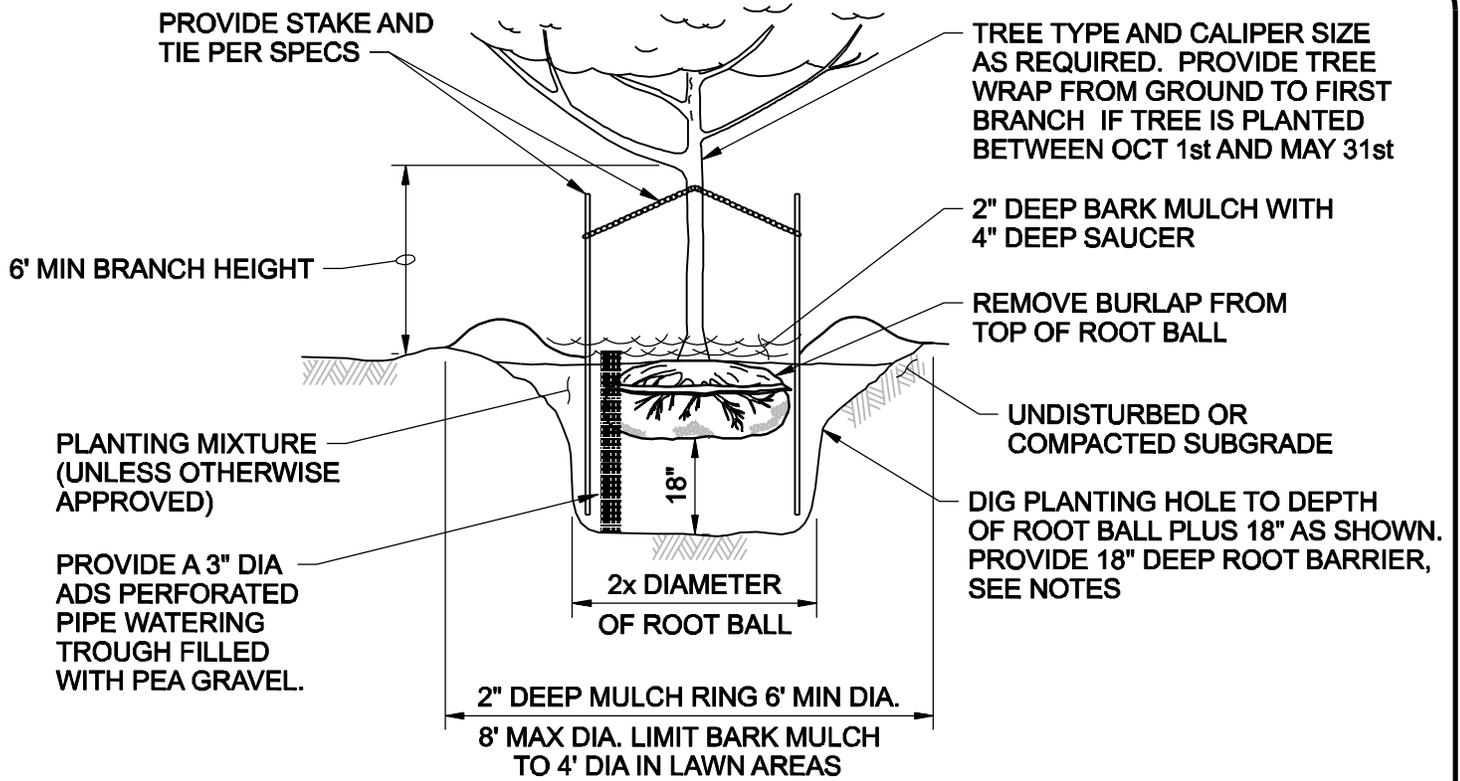
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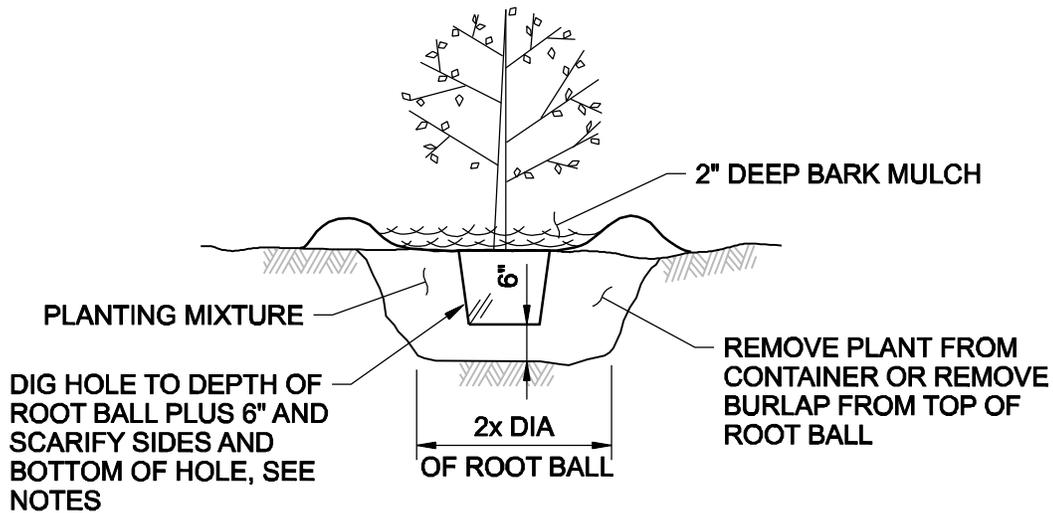
STREET

DRAWING NO.

366



STREET TREE PLANTING DETAIL



SHRUB PLANTING DETAIL

NOTES:

1. IF DRAINAGE IS POOR, ELEVATE ROOT BALL 2" ABOVE GRADE AND SLOPE PLANTING MIXTURE AWAY FROM TRUNK. FORM A 4' DIAMETER SAUCER AROUND TRUNK. UNDER NO CONDITIONS SHOULD FILL BE PLACED IN CONTACT WITH TRUNK OR TOP OF BALL BE EXPOSED.
2. PLANT GROUND COVER AND SHRUBS IN ALTERNATING STAGGERED ROWS.
3. AN 18" DEEP APPROVED ROOT BARRIER SHALL BE PLACED AROUND EDGE OF STREET TREE PLANTING HOLE.



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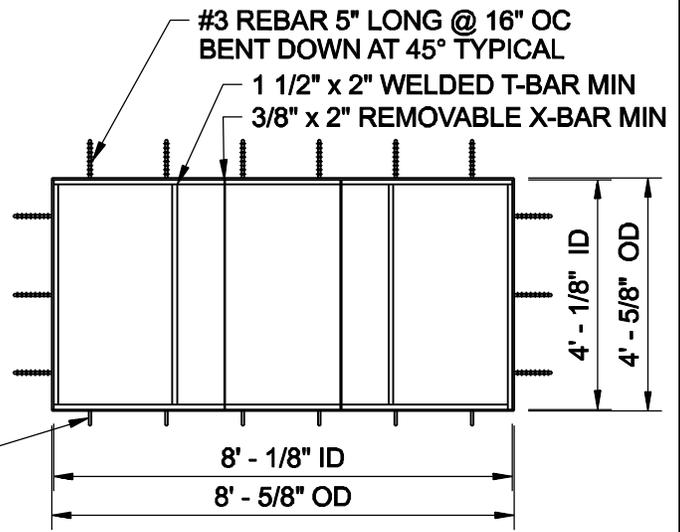
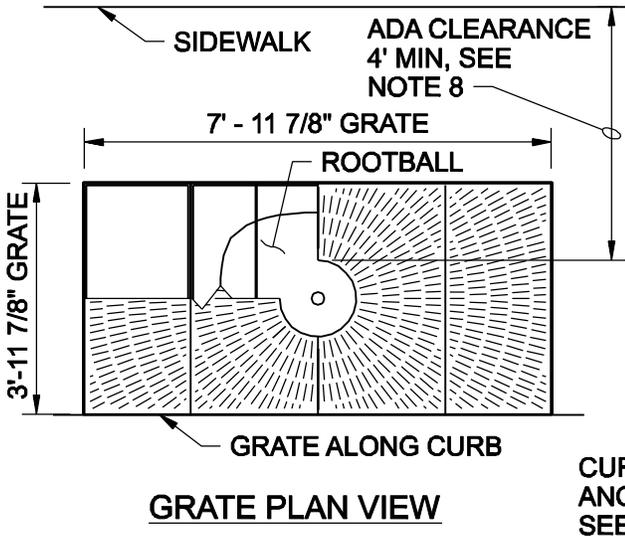
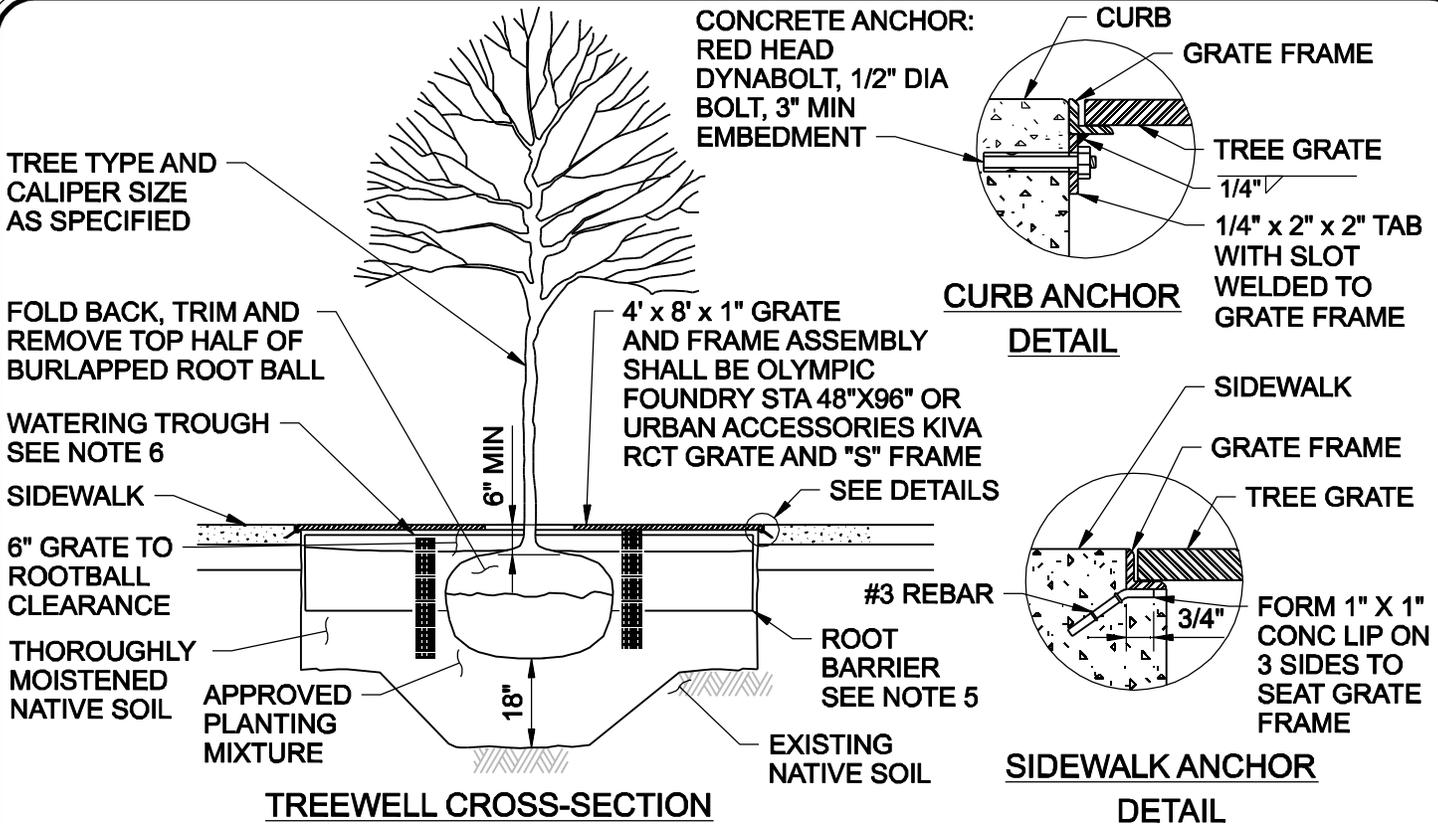
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STANDARD DRAWING TITLE

STREET TREE AND SHRUB PLANTING

NO SCALE
 DIVISION
 STREET
 DRAWING NO.

368



NOTES:

1. GRATES ARE TO BE MADE OF CAST IRON WITH NATURAL FINISH.
2. CASTINGS WILL BE 3/4" THICK IN FOUR PIECES.
3. THE CENTER OPENING SHALL BE 16" DIA WITH A BREAKOUT AT 23 1/2".
4. SLOTTED PENETRATIONS SHALL BE NO GREATER THAN 3/8" WIDE.
5. AN 18" DEEP APPROVED ROOT BARRIER SHALL BE PLACED AROUND PLANTING HOLE.
6. PROVIDE TWO 4" DIA ADS PERFORATED PIPE WATERING TROUGHS FILLED WITH PEA GRAVEL.
7. DO NOT UNDERMIND CURB OR SIDEWALK WHEN EXCAVATING.
8. ADA CLEARANCE SHALL BE 4' AS SHOWN, UNLESS OTHERWISE PERMITTED BY CITY ENGINEER.
9. SEE STANDARD DRAWING 372 FOR IRRIGATION DETAILS.



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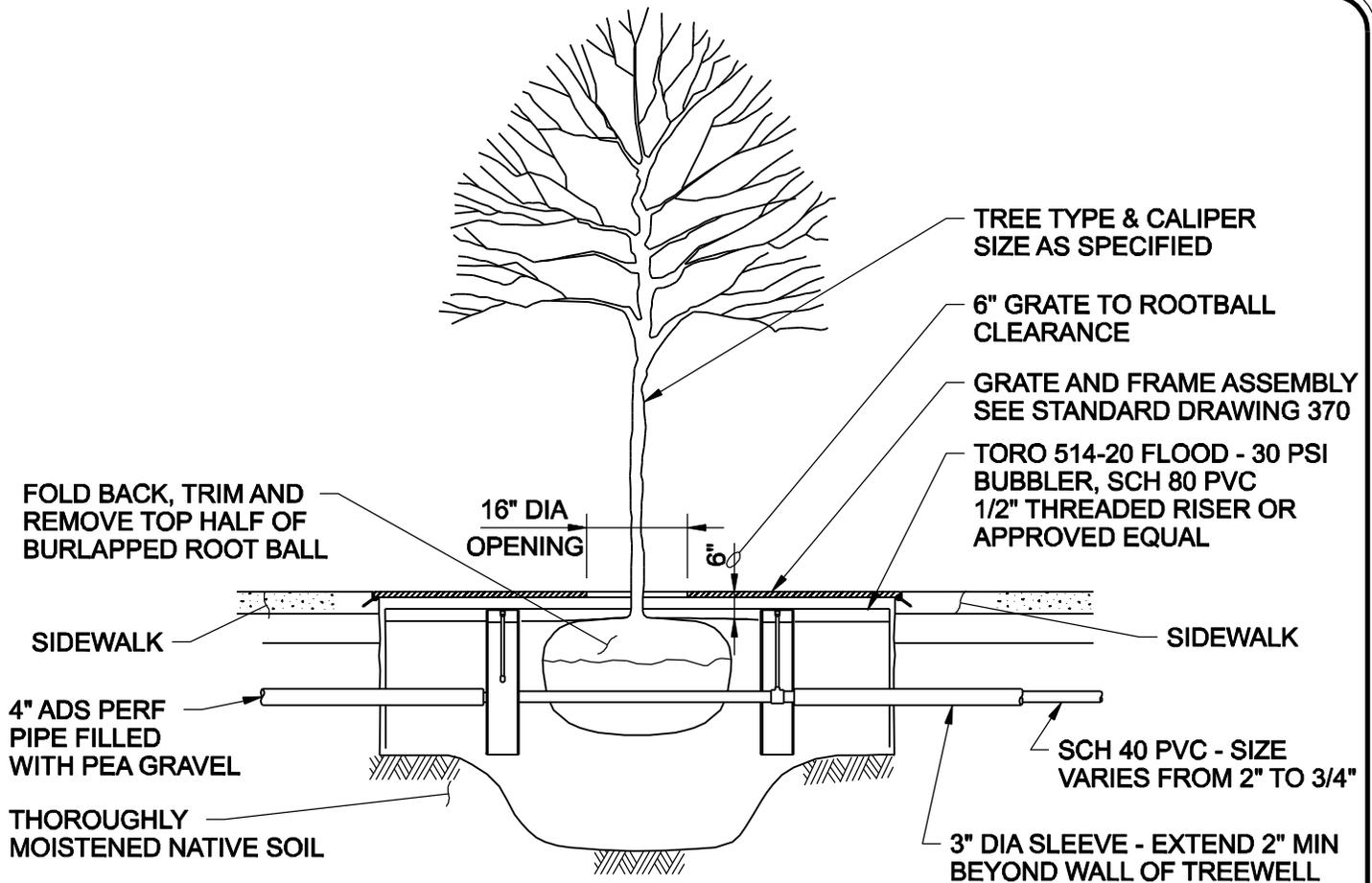
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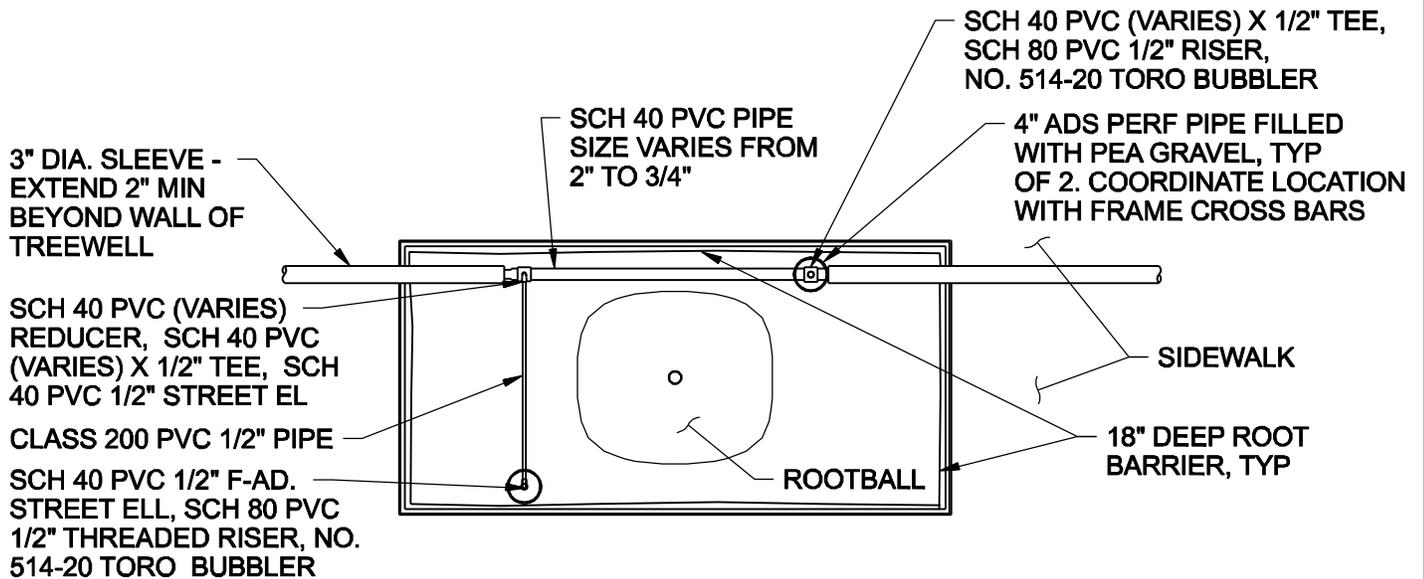
**DOWNTOWN STREET
TREE WELL
GRATE DETAIL**

NO SCALE
DIVISION
STREET
DRAWING NO.

370



TREEWELL CROSS-SECTION



TREEWELL PLAN VIEW



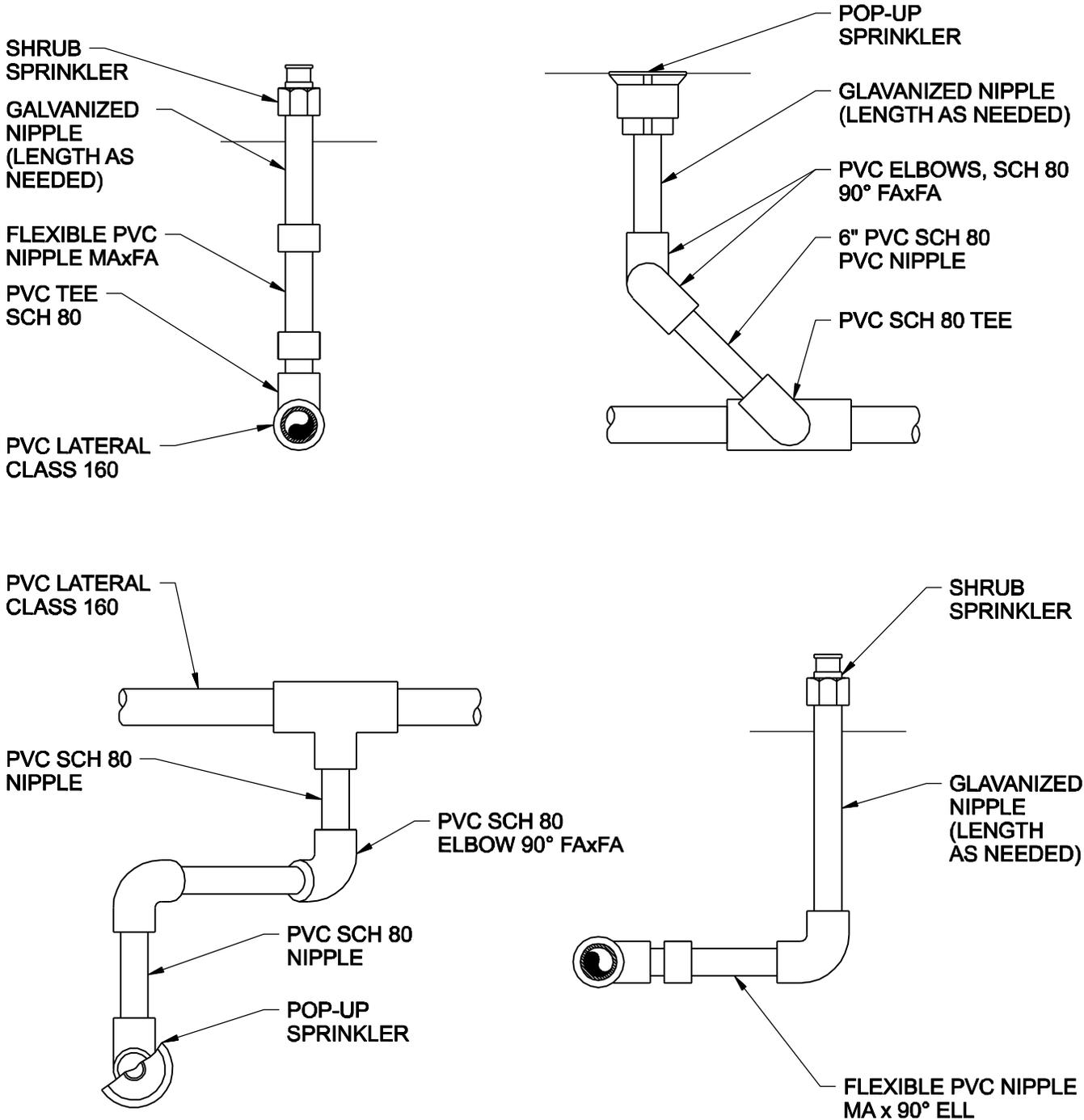
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STANDARD DRAWING TITLE
**DOWNTOWN STREET
TREE WELL IRRIGATION
SYSTEM**

NO SCALE
DIVISION
STREET
DRAWING NO.
372



NOTES:

1. ALL PVC NIPPLES SHALL BE THREADED TYPE.
2. USE TEFLON TAPE ON ALL THREADED CONNECTIONS.
3. ON SOME APPLICATIONS, NIPPLES WITH ELBOW ARRANGEMENT MAY BE REPLACED WITH PVC ELBOW HAVING MAxFA THREADS.



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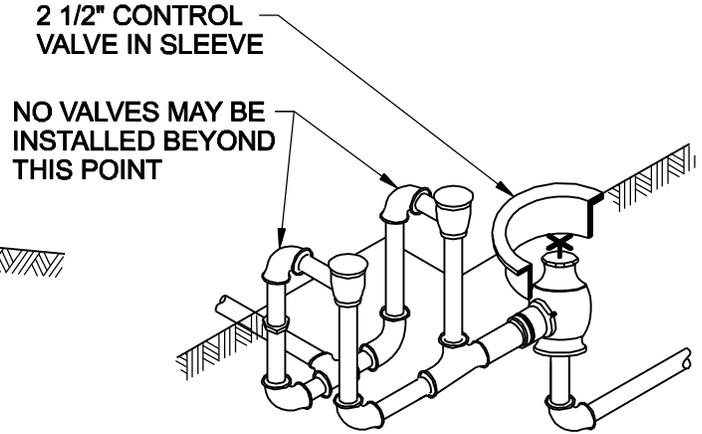
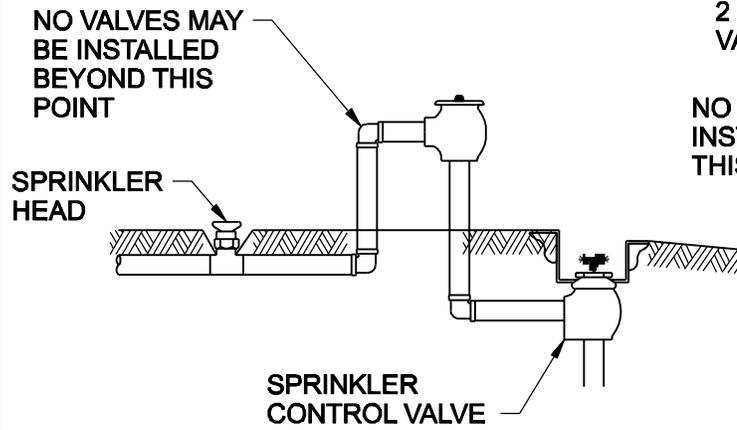
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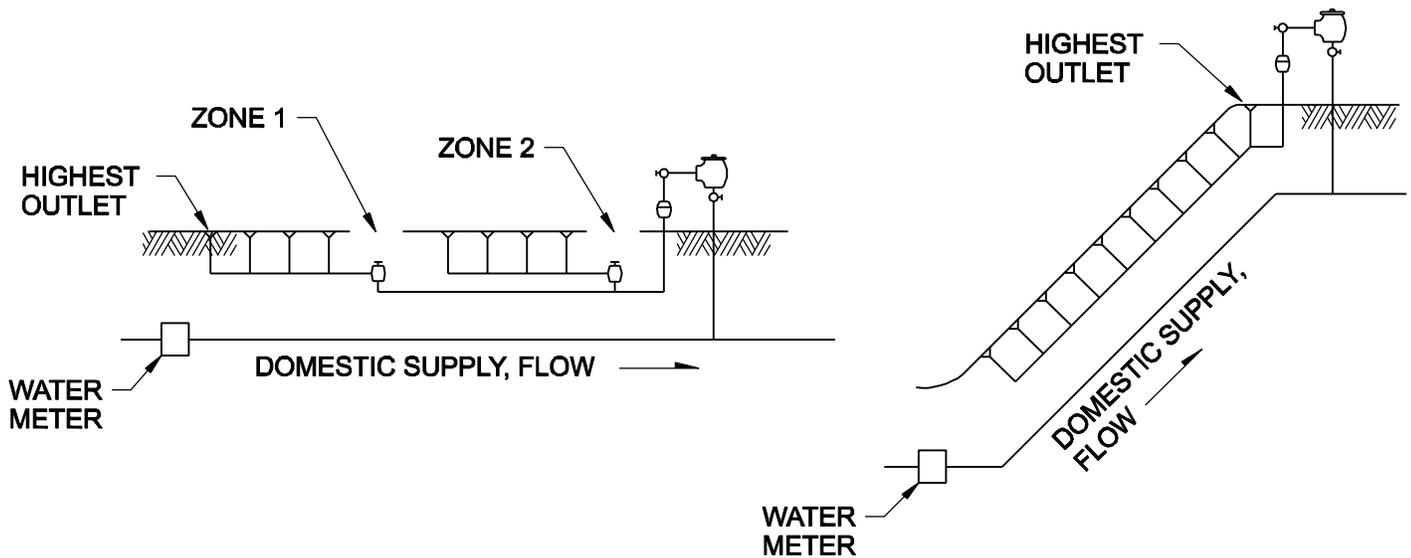
STANDARD DRAWING TITLE

**IRRIGATION SYSTEM
SWING JOINT
RISER ASSEMBLY**

NO SCALE
DIVISION
STREET
DRAWING NO.
374



**ATMOSPHERIC VACUUM BREAKER
(TYPICAL INSTALLATIONS)**



**PRESSURE TYPE VACUUM BREAKER
(TYPICAL INSTALLATIONS)**

NOTES:

1. DEVICE MUST BE TYPE APPROVED BY THE OREGON DEPARTMENT OF HUMAN SERVICES.
2. BOTH ATMOSPHERIC AND PRESSURE TYPE VACUUM BREAKERS MUST BE INSTALLED IN A POSITION ABOVE THE HIGHEST OUTLET BEING SERVED IN ORDER TO PREVENT BACK SIPHONAGE. BREAKERS SHOULD NOT BE INSTALLED IN A PIT OR VAULT WHERE THEY MAY BECOME FLOODED. APPROVED TYPE REDUCED PRESSURE PRINCIPLE OR DOUBLE CHECK ASSEMBLIES MAY BE USED UNDER CERTAIN CONDITIONS WHERE IT IS NOT POSSIBLE TO INSTALL VACUUM BREAKERS ABOVE THE POINT OF HIGHEST USE.
3. THESE DEVICES ARE NOT APPROVED FOR SYSTEMS USING CHEMICAL OR PRODUCT INJECTION OR MIXING SYSTEMS.



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STANDARD DRAWING TITLE
**IRRIGATION SYSTEM
BACKFLOW
PROTECTION**

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DIVISION
STREET
DRAWING NO.
376